

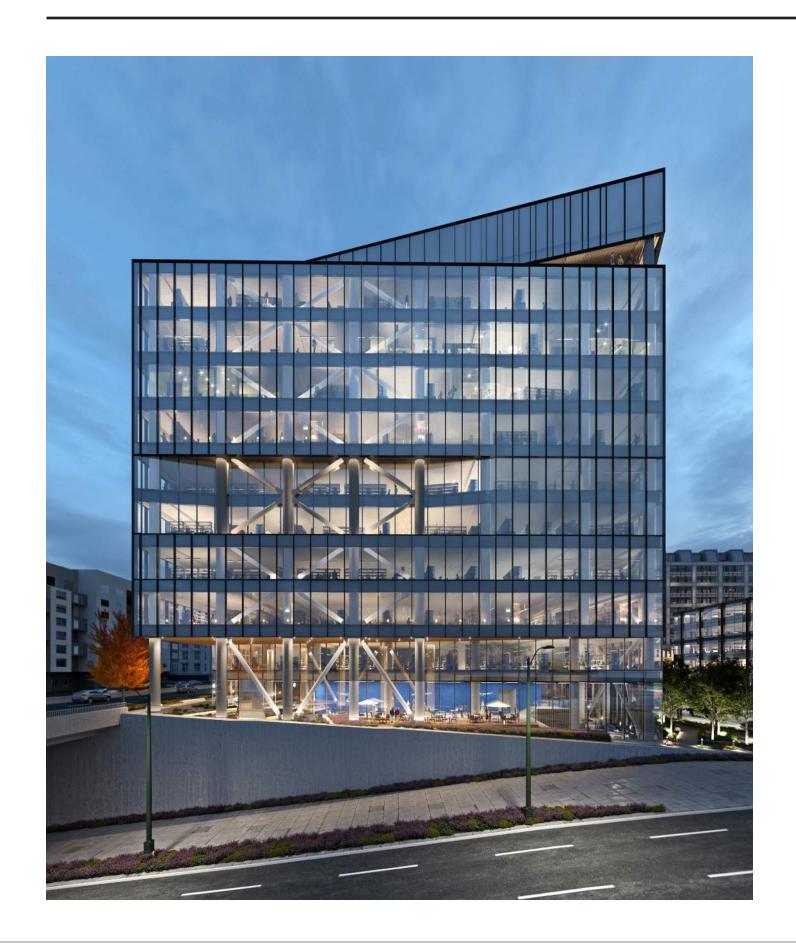
601 DEXTER

Recommendation Meeting project # 3035471-LU #3035375-EG

601 Dexter Ave N Seattle, WA 98109

05.04.2022

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PROJECT OVERVIEW 0



PROJECT DESCRIPTION

Number stories:	11 stories
Gross floor area:	274,669 SF
Number of units:	N/A
Uses per code:	Lab Research
Amount of Parking:	182 stalls (3 underground levels)
Demolition:	Existing 1 story warehouse to be demolished





PROJECT OVERVIEW

A new 11-story lab research building will be located at 601 Dexter Ave N between Aurora Ave N and Dexter Ave N. This project seeks to anchor an important gateway corner in South Lake Union through responsive massing, activated edges, and strong pedestrian experiences.

601 Dexter's primary goal is to offer visitors, residents, and professionals a visual invitation to the world of scientific research. Taking cues from adjacent context, the project relates to street and neighborhood conditions to create a building that expresses science at multiple city scales.

The proposed development consists of:

- One 11-story structure of approximately 274,668 SF above ground purpose design, mission critical lab space
- A ground-floor work lounge with adjacent terrace
- Prominent bicycle entry, workshop and storage
- Highly adaptable floor plates with high floor-to-floor heights
- Balconies and outdoor tenant terraces
- 3 levels of below-grade parking

SITE CONDITIONS

Information

Address:

601 Dexter Ave N, Seattle WA 98109

Owner's name:

ARE-SEATTLE NO 32 HOLDING LLC

Legal description:

EDEN ADD LESS ALLEY LESS ST

Parcel number:

224900-0100

Site area: 23,282 sq.ft.

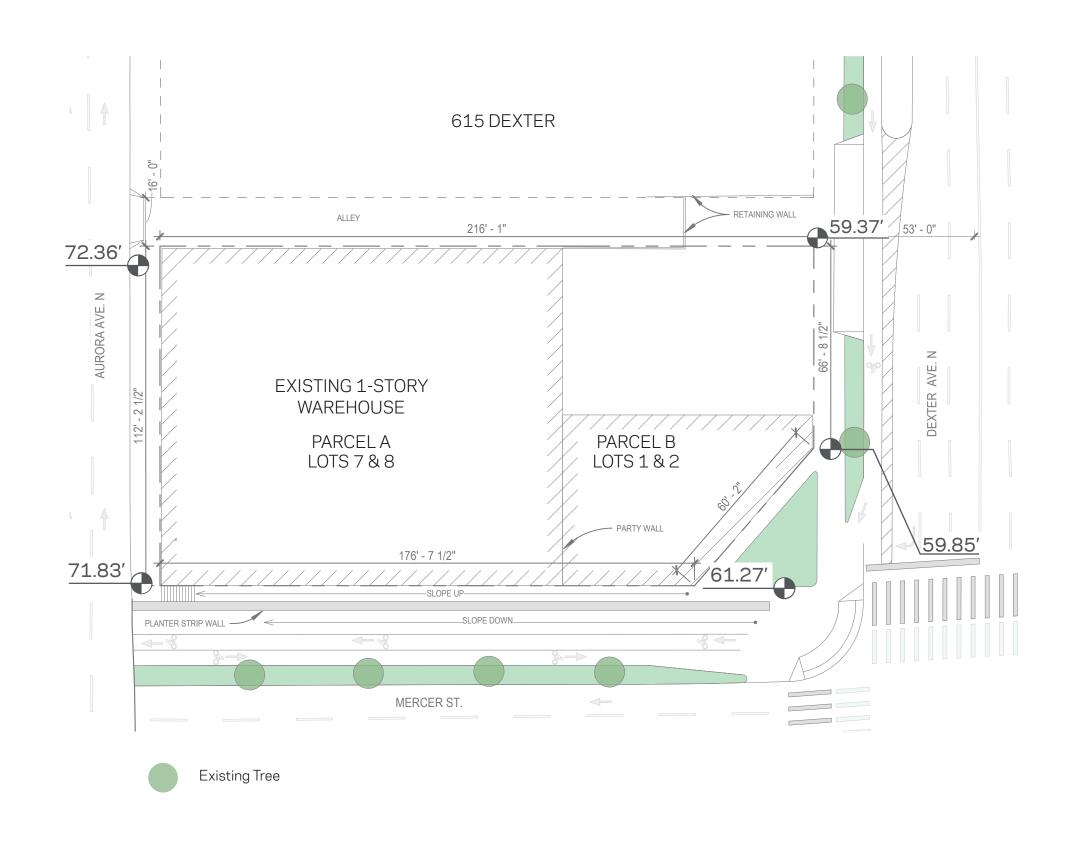
Zoning: SM - SLU 175/85 - 280

Max. FAR: 8.0

Max Height: 175'

Existing Conditions

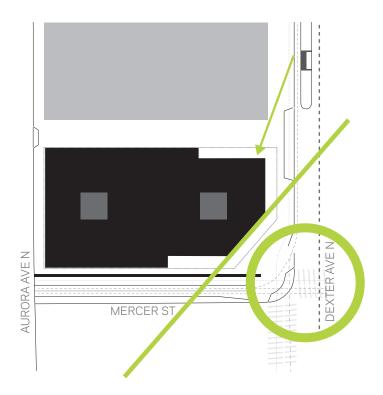
- Vehicular access to site is from Dexter Ave N and through the alley on Aurora Ave N.
 Alley that is accessible through Aurora is not currently connected to Dexter Ave N.
- Protected bike lane starts on the Mercer Ave N.
- Activity is pushed towards Mercer by providing protected bike lane and accessible pedestrian sidewalk.
- Sidewalk to the south of the property connecting Dexter Ave N to Aurora Ave N is not an accessible means of connection.
- South Lake Union Sea Plane Flight Path cuts across the site, limiting overall building height, cores, and mechanical equipment



EDGRESPONSES 02

EDG SUMMARY

OPTION 1 Urban Corner (code compliant)

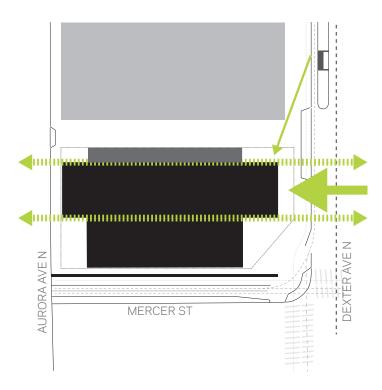




MARK

in direct response to the intersection of Mercer and Dexter
- MARKING the gateway presence by building a continuous
corner facade frontage

OPTION 2 Stacked Volumes



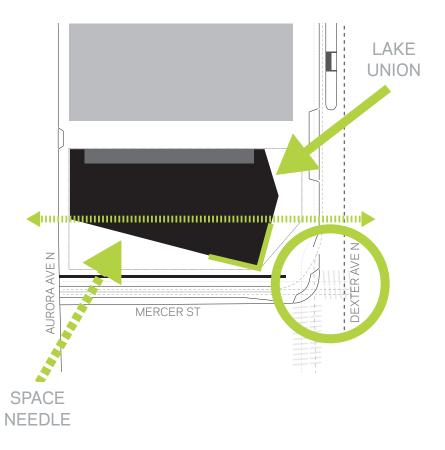


proportioned massing that stacks and slides to TRANSITION between the larger South Lake Union massing and program to the south and east and the smaller Queen Anne massing and program to the north and west

OPTION 3

City Gateway

EDG approved concept



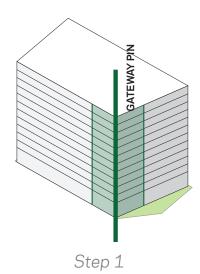


a layered response to the city's context that MARKS the Mercer and Dexter gateway with a strong vertical corner, TRANSITIONS between adjacent scales with proportioned carves within the massing, and SIGNIFIES change both locally and within the larger city context through intentional mass differentiation

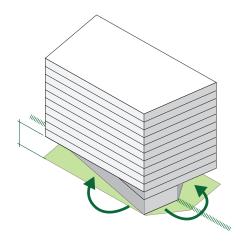
EDG SUMMARY

The City Gateway massing responds to both city and neighborhood at multiple levels and scales. At the ground plane, the massing maintains the important gateway presence while pushing inward at both the northeast and southwest to reinforce the corner while providing improved openness along Dexter and Mercer. A similar gesture happens at the top of the building, creating a noticeable transition in scale to the neighborhoods to the north and establishing desirable outdoor spaces for researchers. The middle of the massing establishes an important urban edge along Mercer and a setback relief along Dexter.

EDG MASSING DEVELOPMENT

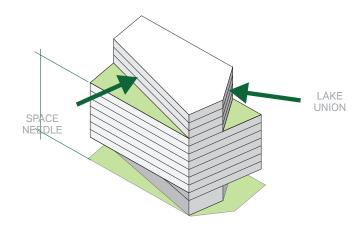


keep a strong vertical presence on the corner of dexter and mercer to **MARK** the gateway entrance and **SIGNIFY** a South lake Union identity



Step 2

divide the massing proportionally, **TRANSITIONING** between the different scales and using that to carve angles away from the "gateway pin" that welcome pedestrians at the ground plane



Step 3

INVITE the larger city and activate a typically static upper part of the building, SIGNIFYING a three-dimensional gateway



- (1) All three contextual analysis summary findings are represented in the preferred massing:
- 2 Establish a strong, continuous vertical presence and rotate carved open spaces to MARK THE GATEWAY INTERSECTION at Dexter / Mercer corner while creating outdoor rooms that improve the ground plane experience for all speeds of traffic
- 3 Lift and carve to **TRANSITION MASSING SCALES** between the smaller residential / local retail of Queen Anne and the larger science / tech of South Lake Union
- (4) Carve upper and mid-level decks that relate to the larger city context (Space Needle and Lake Union, respectively) to create a SIGNIFICANT THREE-DIMENSIONAL GATEWAY mass that invites the city's context in across all scales

EDG COMMENTS AND RESPONSES

Summarized Comments from EDG #1 Report on January 22, 2020:

Design Response Summary

1. Architectural Concept and Massing

a. a. The Board discussed the massing options, agreeing that out of the 3 options, the preferred option was the most rooted in the context, as well as being the most interesting design. The Board provide the following observations and guidance on the preferred option massing:

i. Overall. The Board supported the concept of a pin, at the corner of Mercer street and Dexter Ave, and rotating the massing from that point which provided the potential for a dynamic gateway expression.

ii. Mercer Street. The Board supported pulling back the massing along Mercer Street to create an outdoor room and a unique response to the significant elevation change from Mercer Street to the project site rather than just holding the building to the lot line along this edge.

iii. Aurora Ave. The Board appreciated the design team's acknowledgment of the scale transition to the NW and noted there was merit to narrowing the upper massing facing Aurora Ave by setting back at the upper levels along Mercer Street, which provided some relief to the NW and had potential to create a strong gateway condition. However, the Board was not completely convinced the narrowing of the upper massing at the NW corner fully addressed transitioning to the smaller scale toward the NW. Moving forward the design should continue to evolve to create a thoughtful and intentional response to this context condition.

iv. Alley. The Board supported placement of the core and stair tower, however, echoed public comment, noting more information is needed to illustrate how material application and detailing of the Alley facade will respond to the residential building across the alley. At the next meeting the Board expects to see thoughtful material application, privacy studies, sections, and a clear response to addressing any blank walls.

The massing retains the corner 'pin' as supported by the board, rotating to reveal landscaped spaces that reinforce the dynamic gateway expression. Additionally, the expression at the 'pin' has been enhanced through balconies that bring scale and activity to the corner. Refer to sheet 16, 41,48,49, and 50.

The massing approved by the board has been further developed to strengthen the experience along Mercer. The two-story overhang provides a civic-scaled presence for the bicycle parking entry, an outdoor room accessible from the work lounge, and landscape supporting the project's water detention system. The interior program steps with the topography to ensure activity happens along the street edge. Public access between Dexter and Aurora occurs adjacent to this landscape along the southern edge. Refer to sheet 14, 17, 44-47, 56-57, 68-71, and 81.

At EDG, the preferred proposal addressed massing by reducing the building width on the lower and upper stories by 40% of its possible zoning envelope width. The design continues this desirable scale mitigation along Aurora through three moves that respond to the Board's comments. The first is a massing shift from the southwest face to the back alley, breaking the planar nature of the façade and transitioning in scale to the north. This move reinforces verticality and massing differentiation at the corner of Aurora and Mercer while enhancing views from the units at 615 Dexter toward Seattle Center. The second response is achieved by expressing the structure and stair as human-scaled elements that further break down the massing while relating to the industrial cross-bracing language of the adjacent residential tower. This expression happens along the ground floor and over half of the Aurora elevation. The third response is stressed through vertically proportioned frames that produce a slimming effect to reduce the scale feel of the building. These frames are tightened at the bend in the mass to further emphasize the fold and denote entry. Refer to sheet 18-19, 57, and 73-76.

The core and the stairs have been carefully placed to respect the privacy of the adjacent residential development while creating depth, activity, and visual interest at Dexter and Aurora. The elements expressed are used solely for movement, meaning people working are not staring into spaces for living. The stairs sit to the corners of the north façade to reduce the facade length and promote transparency where it is welcomed. Elevator lobbies open to windows that view north, providing additional depth and glass in the north façade, but in a transitional area. The remaining area of the north façade includes vertical translucent glazing to provide daylight to the toilet rooms, louvers for the mechanical requirements of the tower and carefully detailed metal panel to provide visual interests. Refer to sheet 20-22 and 77-78.

EDG COMMENTS AND RESPONSES

Summarized Comments from EDG #1 Report on January 22, 2020:

Design Response Summary

v. Living Building Pilot and Additional Massing.

1. Regarding the potential for additional massing and height the Board commented the overall massing concept and form could work. However, the additional massing could create height, bulk, and scale impacts. The Board provided guidance to consider how proportions and massing refinements may be warranted with any additional height/massing. If the additional height is pursued, at the next meeting the Board requested illustration of study and analysis be done to accompany the refined massing.

N/A - The Living Building Pilot is no longer being pursued on this site.

2. In addition, the Board commented they would expect to see some expression of the sustainability elements in the design and clarity on how this apparent in the building design.

A healthy and sustainable work environment is expressed through the massing with deep cuts letting nature in at the ground plane and elevated terrace. It is also promoted through the expression of balconies on the SE corner and transparent bike parking surrounded by lush planting on the SW Corner. Refer to sheet 23, 44-47, and 48-50.

2. Ground Plane

a. Overall the Board commended the design team for the unique massing at ground level which carved the massing along both Mercer Street and Dexter Ace to create active spaces at the ground level.

The approved carves in the massing have been reinforced through various programs that ensure the ground level remains active. The main lobby and reception is directly adjacent to the Dexter entry, and the elevators and stair are highly visible and accessible from this space. A terraced work lounge and mezzanine wrap the southeast corner of the project, transitioning with the grade and energizing the Mercer experience. The corner of Aurora and Mercer has been anchored by a highly-transparent bike storage and workshop area. Refer to sheet 24-25, 44-47, and 90-91.

b. However, the Board had some reservations on the ground plane's lack of connection between Dexter Avenue and Aurora Ave, as this seemed like a missed opportunity to further improve circulation around the building. At the next meeting the Board requested further evolution and study on how this project could enhance accessibility and circulation between Aurora Ave and Dexter Ave.

To improve the circulation and connectivity between Dexter and Aurora along Mercer, the sidewalk has been recreated as a more gradual ramp leading to stairs at Aurora. This ramp also serves for cyclists accessing the visible bike support areas from the cycle track. The corner at Aurora opens up by almost 40' to create a well-scaled welcome for pedestrians whether approaching from the north or south. Refer to sheet 24-25, 81, and 83.

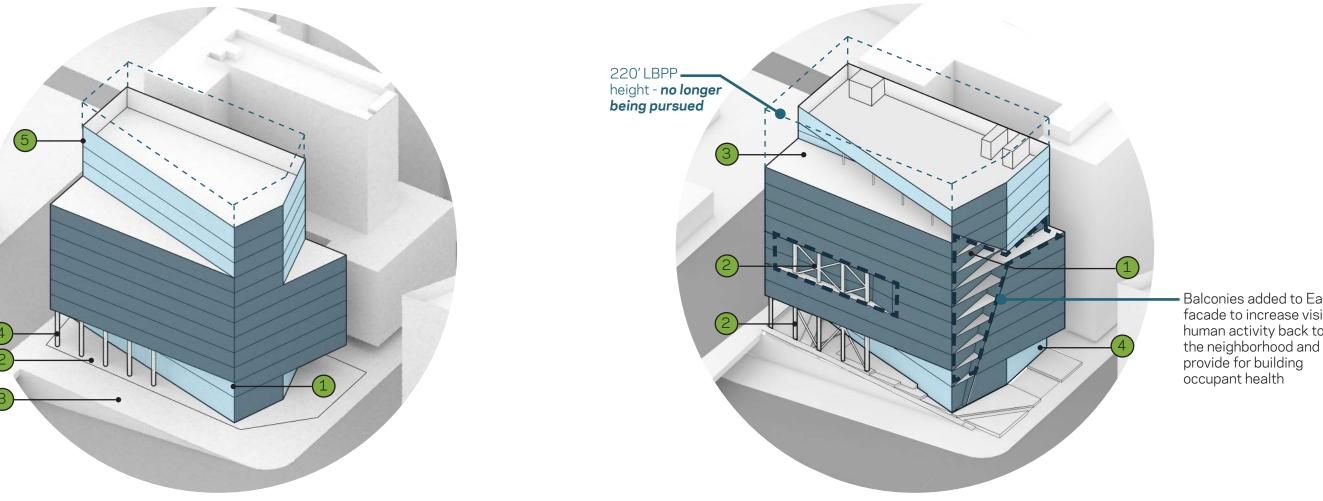
c. The Board discussed the corner of Dexter Ave and Mercer Street, commenting that though they appreciated the generous amount of landscaping, they questioned if there could be further engagement with the public. This is a very busy corner, so perhaps more occupied space would be more appropriate. However, no specific solution was recommended. The Board requested studies be provided at the next meeting, illustrating the refinement of the corner with the goal of improving connectivity, circulation, and public engagement. The Board requested the following information and design responses.

During EDG, the board's discussion of the intersection of Dexter and Mercer considered the balance between framing of the intersection with planting as proposed to mitigate the overall extent of paved area and the potential for greater pedestrian engagement by enlarging the paved area. The revised design enhances the potential for public engagement at the corner of Dexter and Mercer by adding a layer of seating and pedestrian passage into the sloping site along with opening up the corner. The new pedestrian pathway and seating frames the public ROW where a grove of Alders with native planting below is supported by SDOT as a sustainability feature in this unique urban condition. The additional layer of planting and trees within the SDOT property maintains accessible slopes through the intersection and frames the intersection. The revised design shifts the orientation of the planting edge to widen the sidewalk between Dexter and Mercer and more equally frame the intersection to allow one of the feature benches to better serve pedestrians crossing either street.

EDG COMMENTS AND RESPONSES

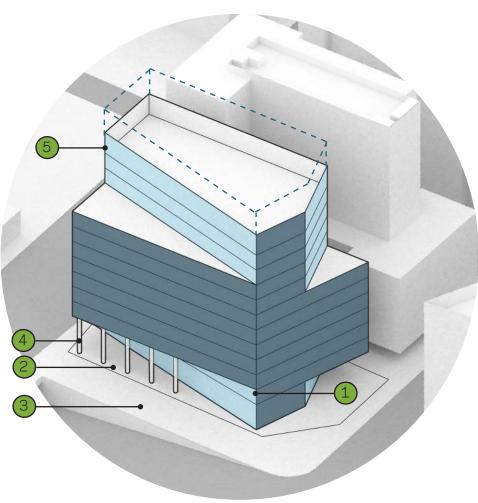
Summarized Comments from EDG #1 Report on January 22, 2020:	Design Response Summary	
i. Analysis on how the site is used currently and more clarity on how the design team envisions this site functioning moving forward.	See circulation diagrams with before and after on sheet 26.	
ii. Continue to evolve the ground plane and gateway concept and the way people pedestrians, cyclist, automobiles) move through and around the project.	The revised mercer sidewalk grading to incorporate a sloped ramp allowing for the removal of the existing stairs. A cut-through path at the south east corner provides direct access to the Upper Mercer sidewalk while maintaining a vegetated buffer for pedestrians at the intersection. Refer to sheet 27.	
iii. Consider how topography can inform decision making regarding landscaping and site circulation refinements.	The Wedge planting utilizes falling natural grade from Aurora to Dexter to enhance the cascading bio-retention zones. As one cell fills up it can then cascade into the adjacent cell, reminiscent of the natural water hydrology. Refer to sheet 27 and 82.	
iv. Clarify if the intent is for the landscaping to be a counterpoint to the strong angles of the massing, or for those angles to continue down to the ground plane. Perhaps a softer ground plane could also help mitigate design concerns.	The soft, lush planting of the Landscape provides a contrast to the strong angles of the building massing. This contrast allows the Landscape to feel larger and 'continuous' rather than fragmented and piecemeal. Refer to sheet 28.	
v. Clarify how the columns are integrated into the overall concept and detailed.	The structure brings a neighborhood scale to the project which can be experienced at the wedge and throughout the tower. The exposed concrete brings an honesty of materials experienced from the ground plane up to the downtown terrace. The raw materials support an organic ground plane experience of concrete columns rising from a lush landscape and connecting to the wood soffit above. Refer to sheet 30-31 and 71.	
d. Bicycle Circulation. The Board would like more information on how bike access and safety has been refined to address the conflicts at the alley improve bike access.	Access to bike parking has been moved to the cut along Mercer, creating more activity at the cut and an enhanced cycling experience. A bridge over bio-retention connects bikers to a transparent parking area. Refer to sheet 27 and 83.	
3. Materials and Facade Development		
a. The Board emphasized that as the project evolves with material detailing, they expect to see more than a glass box with fins and recommended reviewing the tall building design guidelines. The Board expects to see thoughtful material application at this gateway corner which includes detailing of secondary architectural elements and depth to provide greater visual interest and character, and further breakdown of the scale of the building.	Dynamic massing moves have been created to carve through the glass box, establishing a series of small and mid-level scales that further break down the massing as seen at EDG. These moves extend across their facades to further introduce depth while bringing activity to the exterior. The Gateway Corner is accented through wood soffits that offer warmth and cohesion along Dexter. The Aurora / Mercer corner pushes back to express a secondary read of structure, further eroding the glass box. The northeast and northwest corners are accented by stairs that provide interest, movement, and a unique expression. Refer to sheet 29, 52-53, and 56-57.	
b. The Board noted consideration of the site's context including proximity to Lake Union may help to inform the material palette and facade development.	Referencing the industrial history of South Lake Union, wood and metal in both the building and landscape reflect materials that have been a part of the area for the last century. Water plays a prominent and visible role in both place making and connection to history through the landscape water detention area along Mercer. Refer to sheet 60-61.	

DEVELOPMENT OF MASSING



Proposed Design

- The Gateway Corner maintains "pin" condition that was supported by the Board. As a response to the Board's concern of scale in the massing and facade the Gateway Corner supports a new medium-scale massing move to introduce further accent and relief through visually linked balconies
- Hearing the boards concern for additional facade expression the structural system for the building is expressed both at the ground plane and in the facade, creating a unique way for pedestrians, cyclists, and drivers to understand the building at differing speeds
- The massing along Mercer has raised slightly to make the building's top floor one that can house building amenities for all tenants. This shift ensures an active terrace visible from the Space Needle, and also further differentiates the urban edge of the Mercer face from the lower massing along Dexter.
- The northeast corner of the ground floor has been rotated to more directly relate to those arriving from Dexter. This move also allows a glassy egress stair to accent the building at the entry and increase the public's engagement with the building at this corner, while also encouraging tenants to take this route to the upper floors.



EDG Approved Massing

- Bolster the corner through architecture, landscape and activity
- Reinforce the outdoor room along Mercer
- Improve the circulation between Dexter and Aurora
- Integrate the structure as part of expression and experience
- Transition the scale to the NW

Balconies added to East facade to increase visible human activity back to

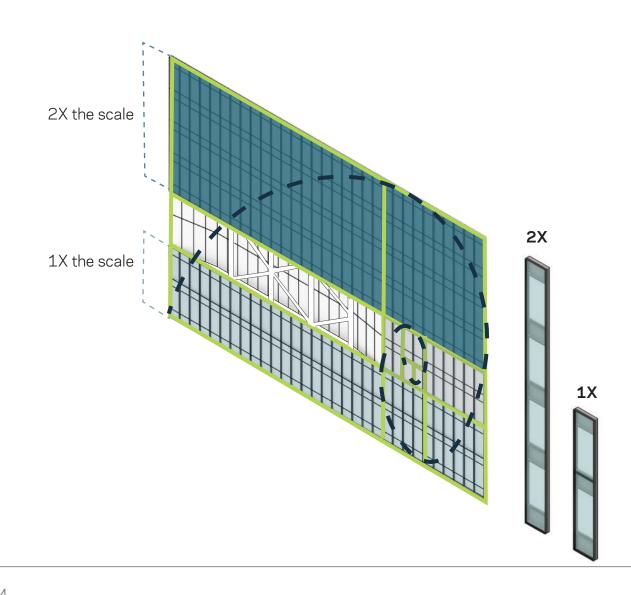
occupant health

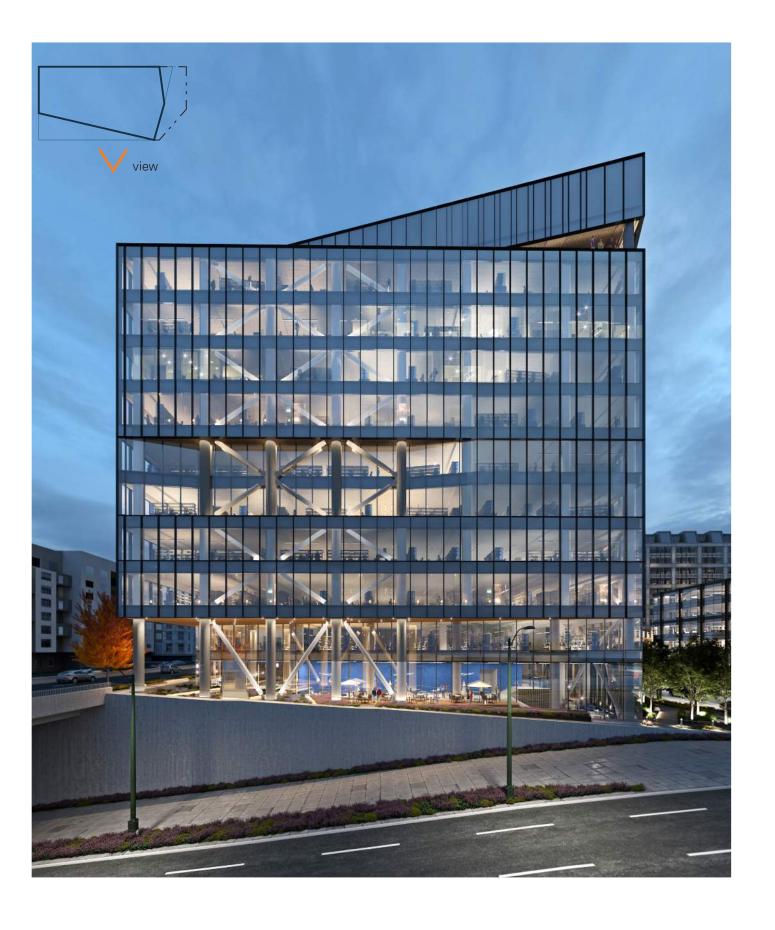
DEVELOPMENT OF MASSING AND FACADE

LEVEL 11 TERRACE

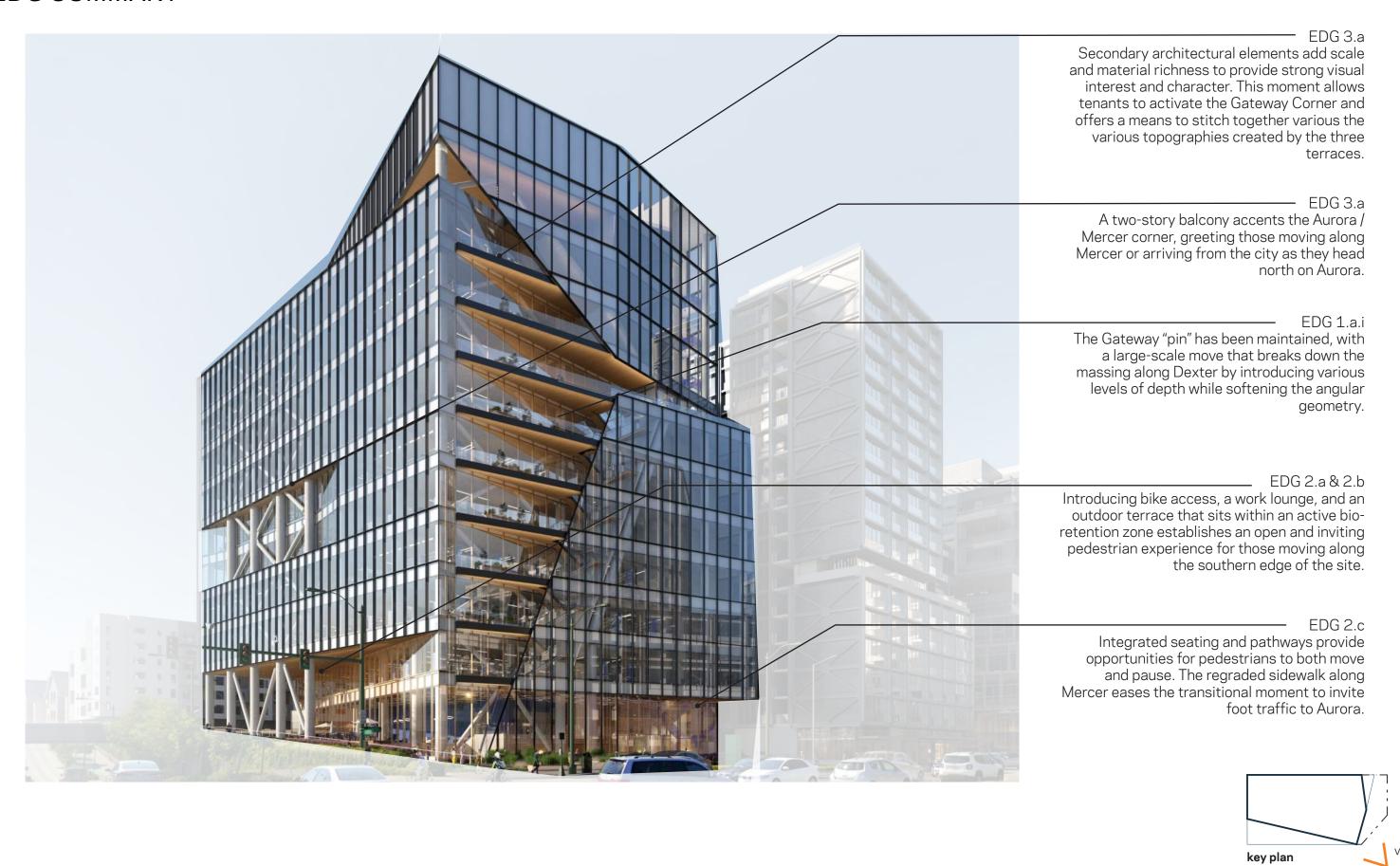
In further evaluating the important massing along Mercer, the design has been improved to reflect the urban role of this property. The increased height of the south facade further solidifies the gateway experience by establishing a stronger urban edge on Mercer and, through the full-building carve, a direct visual connection between terraces at grade, mid, and upper building. Inspired by the golden ratio, this refinement also creates a better proportion along the south façade to provide balance along this primary street. The resultant façade dimensions enable a well-scaled subtraction from L5 and L6 that creates

a balcony to address Aurora and Mercer through expressed structure. Finally, the activation of the upper terrace is maximized; by elevating this amenity to the top floor of the building allows all tenants to potentially use a space that has visual connections from Lake Union, Seattle Center and the Space Needle.





EDG SUMMARY



1. Architectural Concept and Massing

a. The Board discussed the massing options, agreeing that out of the 3 options, the preferred option was the most rooted in the context, as well as being the most interesting design. The Board provide the following observations and guidance on the preferred option massing:

i. Overall. The Board supported the concept of a pin at the corner (of Mercer Street and Dexter Ave) and rotating the massing from that point which provided the potential for a dynamic gateway expression.

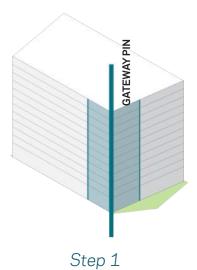
RELEVANT DESIGN GUIDELINES: South Lake Union CS1-Topography, CS2-A-2. Architectural Presence, South Lake Union CS2-1 Gateways Locations CS2-2 Heart Locations. CS1-C Topography, CS2-B-1. Site Characteristics.

RESPONSE

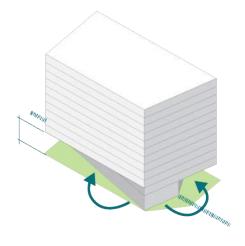
1A. Architectural Concept and Massing

i. Overall. The Gateway Corner retains the "pin" that was supported by the Board. This corner expression is accented by a continuous cut in the massing that emphasizes the collaborative side of science through balconies. The carve creates an iconic moment not only in its physical shape but in the space it creates, establishing an iconic moment that creates a sense of identity and place.

EDG-PREFERRED MASSING CONCEPT

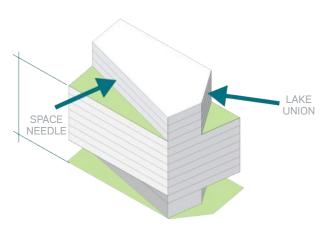


keep a strong vertical presence on the corner of dexter and mercer to **MARK** the gateway entrance and **SIGNIFY** a South lake Union identity



divide the massing proportionally, **TRANSITIONING** between the different scales and using that to carve angles away from the "gateway pin" that welcome pedestrians at the ground plane

Step 2

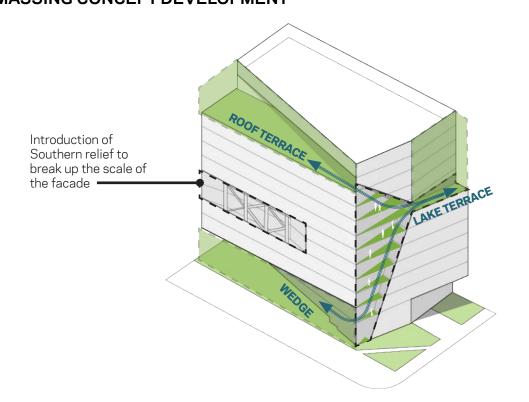


INVITE the larger city and activate a typically static upper part of the building, **SIGNIFYING** a three-

Step 3

dimensional gateway

DRB- MASSING CONCEPT DEVELOPMENT



Step 4

Introducing balconies at the Mercer
Dexter Corner creates **ACTIVATION** along
the gateway corner. The massing move **CONNECTS** the ground floor wedge to
the terrace to the roof terrace.

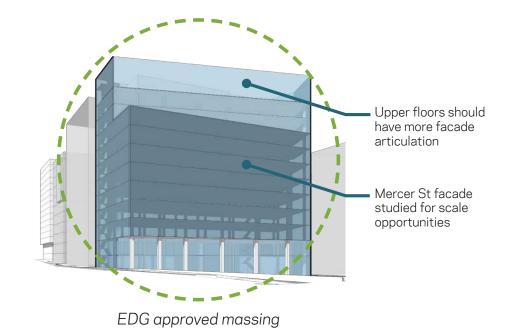
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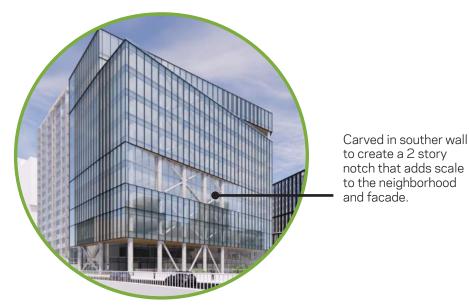
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RELEVANT DESIGN GUIDELINES: South Lake Union CS1-Topography, CS2-A-2. Architectural Presence, South Lake Union CS2-1 Gateways Locations CS2-2 Heart Locations. CS1-C Topography, CS2-B-1. Site Characteristics.

EDG-PREFERRED MASSING CONCEPT



DRB- MASSING CONCEPT DEVELOPMENT



Developed design View from Aurora looking North

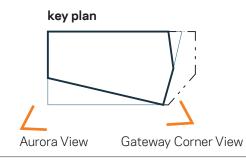
RESPONSE

1A. Architectural Concept and Massing

ii. Mercer Street. The massing approved by the board has been further enhanced to strengthen the experience along Mercer. A two-story overhang provides a civic-scaled presence showcasing the bio-retention system while sheltering an outdoor terrace and bicycle entry. The interior program steps with the topography, meeting the various grades along this edge with activity. Above the terrace, an inset balcony creates a mid-building scale that greets those a few blocks away.



View from Gateway corner to Aurora Ave.





1. Architectural Concept and Massing

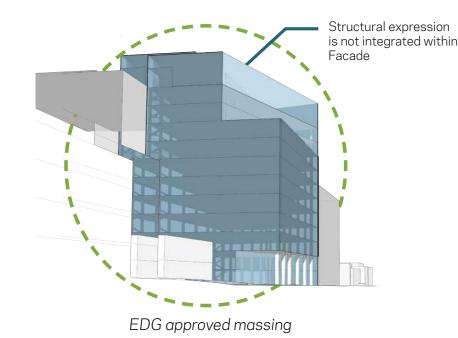
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iii. Aurora Ave. The Board appreciated the design team's acknowledgment of the scale transition to the NW and noted there was merit to narrowing the upper massing facing Aurora Ave by setting back at the upper levels along Mercer Street, which provided some relief to the NW and had potential to create a strong gateway condition. However, the Board was not completely convinced the narrowing of the upper massing at the NW corner fully addressed transitioning to the smaller scale toward the NW. Moving forward the design should continue to evolve to create a thoughtful and intentional response to this context condition.

RELEVANT DESIGN GUIDELINES: CS2-D-1. Existing Development and Zoning, CS2-D-4. Massing Choices, South Lake Union CS2-3-a. Aurora and Dexter Ave N.

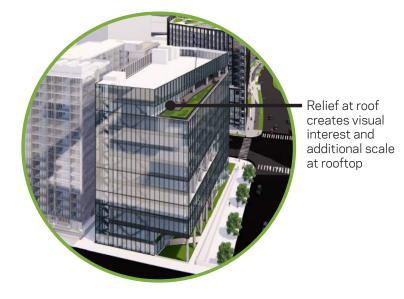
EDG-PREFERRED MASSING CONCEPT

DRB- MASSING CONCEPT DEVELOPMENT

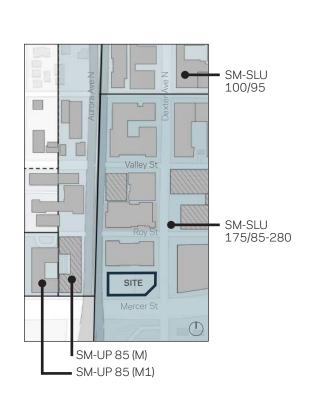


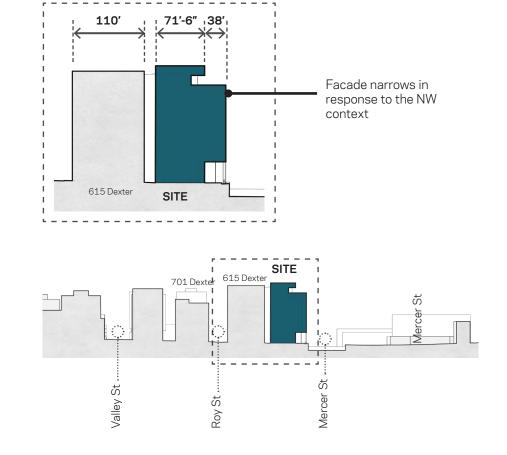
Fold at Ground Level creates

sidewalk



Developed design View from Mercer looking East







Aurora Street View to SW Corner

RESPONSE

1. Architectural Concept and Massing

iii. Aurora Ave. The building massing maintains its primary objective of "slimming" the west facade to express a transition to the smaller-scale communities to the northwest. With 615 Dexter and 701 Dexter being constructed soon, the massing to the north of 601 will be significantly taller; 601 Dexter plays a role in transitioning from street scale to community scale.

At the base and top of the building, the southern corners open to allow pedestrians and residents to look past 601 Dexter to the city beyond; the lower two floors continue this pulled-back language to the northern corner. The massing tapers from the southwest corner to the north to further enhance this shaping.

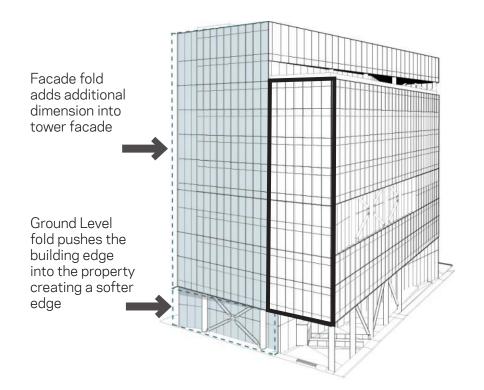
A vertical stair runs the full height of the building; its transparency reduces the overall bulk of the facade by another 20'. Adjacent to the stair is a 40' bay of cross-bracing that offers another level of vertical distinction and a secondary scale along Aurora. A series of vertical frames modulate the facade, proportionally slimming the mass.





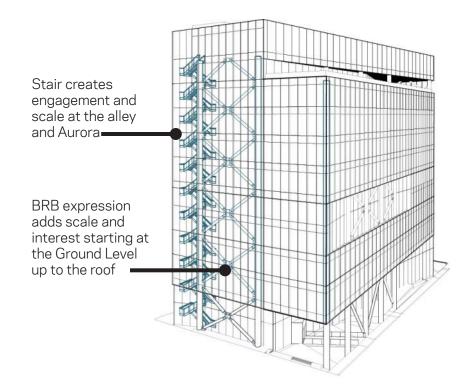
View from Aurora Ave looking South

DRB-MASSING CONCEPT DEVELOPMENT



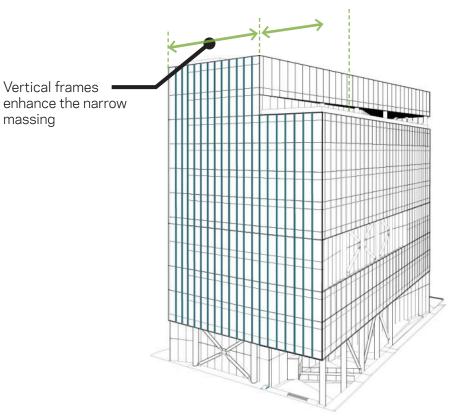
Massing Fold

Folding the massing away from Aurora brings engagement and dimensionality into the elevation, strengthening the pedestrian experience and adding more interest along sidewalk



Structure / Stairs

Showcasing the BRB structure and stairs through the transparent facade adds prominent scale through the transparent facade



Vertical Frames

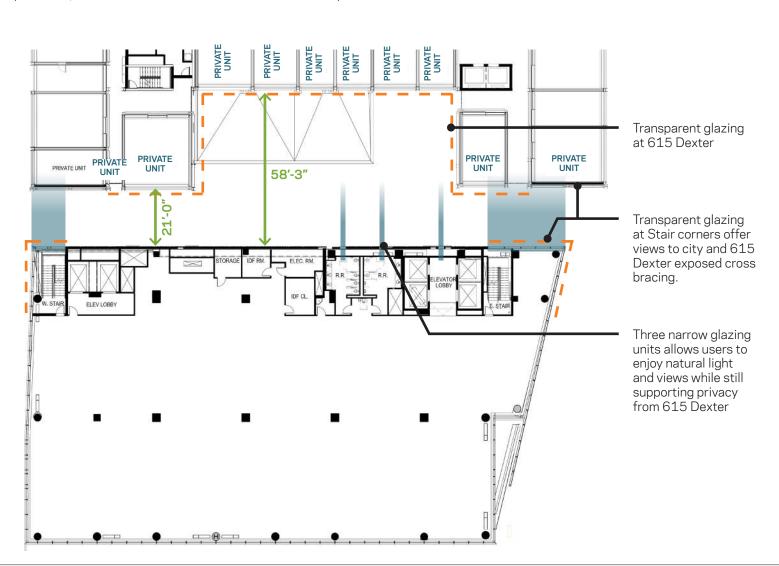
Vertical elements of the frames reduce the scale of the facade, further slimming the profile of the building visually

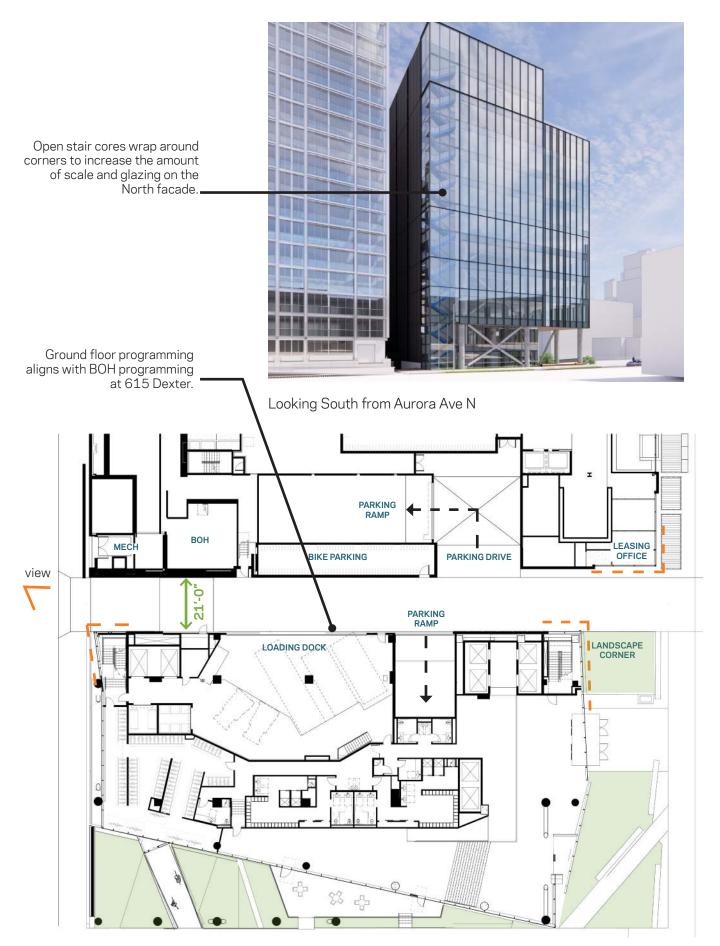
1. Architectural Concept and Massing

a. The Board discussed the massing options, agreeing that out of the 3 options, the preferred option was the most rooted in the context, as well as being the most interesting design. The Board provide the following observations and guidance on the preferred option massing:

iv. Alley. The Board supported placement of the core and stair tower, however, echoed public comment, noting more information is needed to illustrate how material application and detailing of the alley facade will respond to the residential building across the alley. At the next meeting the Board expects to see thoughtful material

RELEVANT DESIGN GUIDELINES: DC2-B-1, Facade Composition, DC2-B Architectural and Facade Composition.





RESPONSE

1. Architectural Concept and Massing

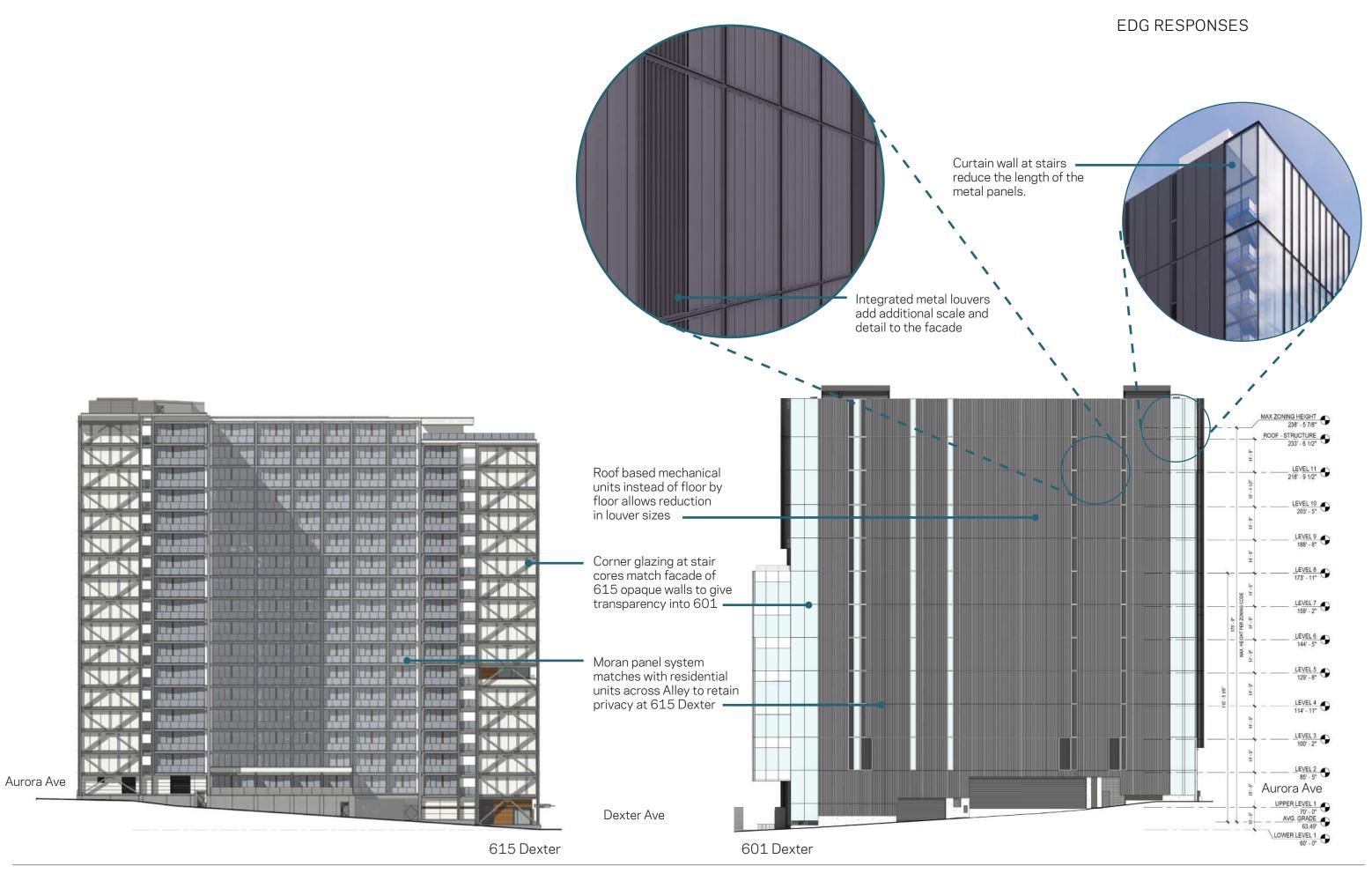
iv. Alley. The core and the stairs have been carefully placed to respect the privacy of the adjacent residential development while creating depth, activity, and visual interest at Dexter and Aurora. The elements expressed are used solely for movement, meaning people working are not staring into spaces for living. The stairs sit to the corners of the north facade to reduce the facade length and promote transparency where it is welcomed. Elevator lobbies open to windows that view north, providing additional depth and glass in the north facade, but in a transitional area. The remaining area of the north facade includes vertical translucent glazing to provide daylight to the toilet rooms, louvers for the mechanical requirements of the tower and carefully detailed metal panel to provide visual interest. Refer to sheets xx-xx.



58'-3" PRIVATE UNIT PRIVATE UNIT PRIVATE UNIT Elevator Lobby distribution areas PRIVATE UNIT Glazing at Elevator Core - Site Study 58'-3" 2'-0" narrow glazing panels run from Level 2 to 11. 186 - 81 D LEVEL 12 159 - 2" 1 DEVEL 10 ◆ LEVEL 9 146' - 0" 144' - 5" ◆ LEVEL 8 129 - 8" 1 € LEVEL 7 125' - 8" ◆ LEVEL 6 115' - 6" 115-11 9 DEVEL 5 100 - 2" S ⊕ LEVEL 4 Alley ⊕ LEVEL 2 75' - 10" ⊕ LEVEL 1.5 615 Dexter 601 Dexter

Looking South from Dexter Ave N

key plan



EDG RESPONSES

GUIDANCE

1. Architectural Concept and Massing

a. The Board discussed the massing options, agreeing that out of the 3 options, the preferred option was the most rooted in the context, as well as being the most interesting design. The Board provide the following observations and guidance on the preferred option massing:

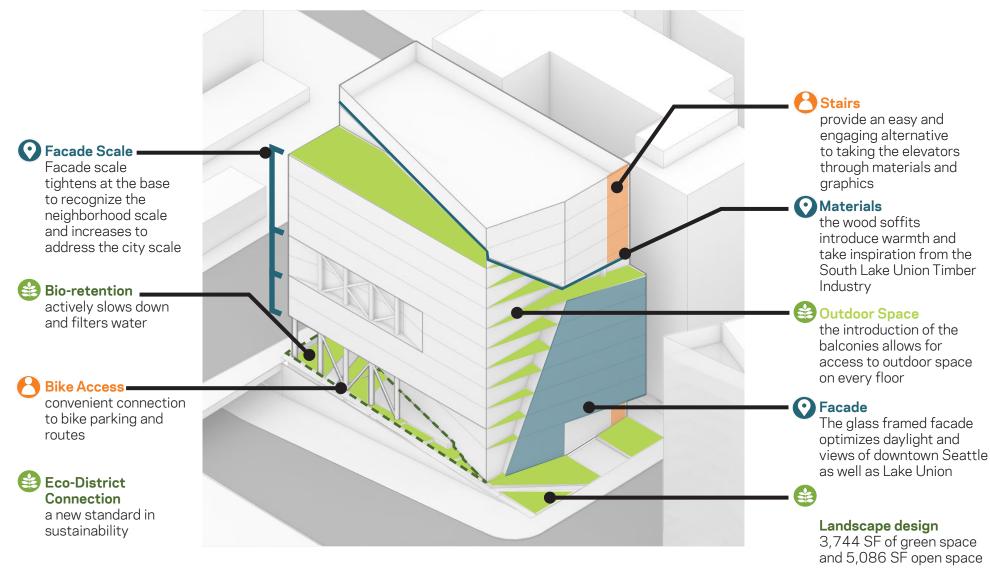
v. Living Building Pilot and Additional Massing.

- 1. Regarding the potential for additional massing and height the Board commented the overall massing concept and form could work. However, the additional massing could create height, bulk, and scale impacts. The Board provided guidance to consider how proportions and massing refinements may be warranted with any additional height/massing. If the additional height is pursued, at the next meeting the Board requested illustration of study and analysis be done to accompany the refined massing.
- 2. In addition, the Board commented they would expect to see some expression of the sustainability elements in the design and clarity on how this apparent in the building design.

RELEVANT DESIGN GUIDELINES: CS2-D Height, Bulk, and Scale, DC2-4 Tall Buildings. South Lake Union CS1-A Energy Use

RESPONSE

- 1. Architectural Concept and Massing
 - v. Living Building Pilot and Additional Massing.
 - 1. LBPP is not being pursued
 - 2. A healthy and sustainable environment is promoted through landscape, material, and activity. Porous landscape and bioretention have an increased presence and are part of the arrival and grade-level outdoor experience. Wood is used as both an accent and support material. Cyclists have been prioritized through program location and access points. Outdoor access is provided on every floor. The building will tap into the larger energy district, a feature being considered as part of a graphic storyboard in the lobby.



DEVELOPING HEALTH AND SUSTAINABILITY AT 601 DEXTER



= activation

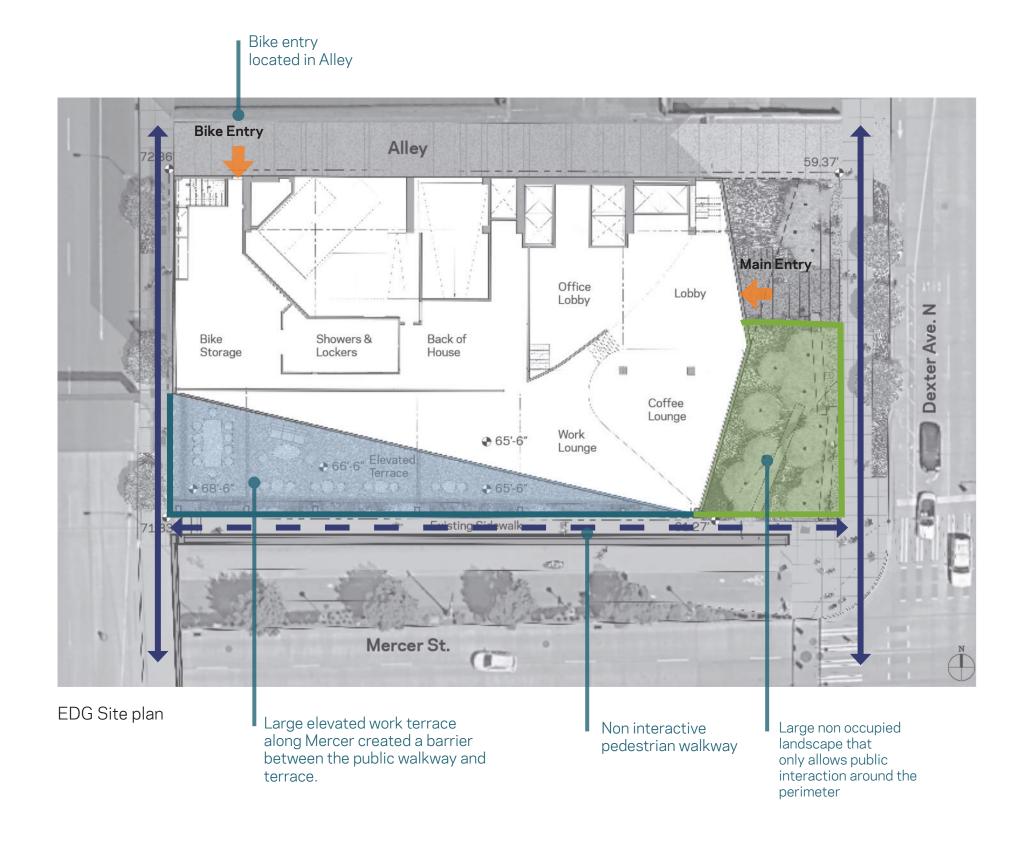
= ecology

= context

2. Ground Plane

- **a.** Overall the Board commended the design team for the unique massing at ground level which carved the massing along both Mercer Street and Dexter Ave to create active spaces at the ground level.
- **b.** However, the Board had some reservations on the ground plane's lack of connection between Dexter Avenue and Aurora Ave, as this seemed like a missed opportunity to further improve circulation around the building. At the next meeting the Board requested further evolution and study on how this project could enhance accessibility and circulation between Aurora Ave and Dexter Ave.

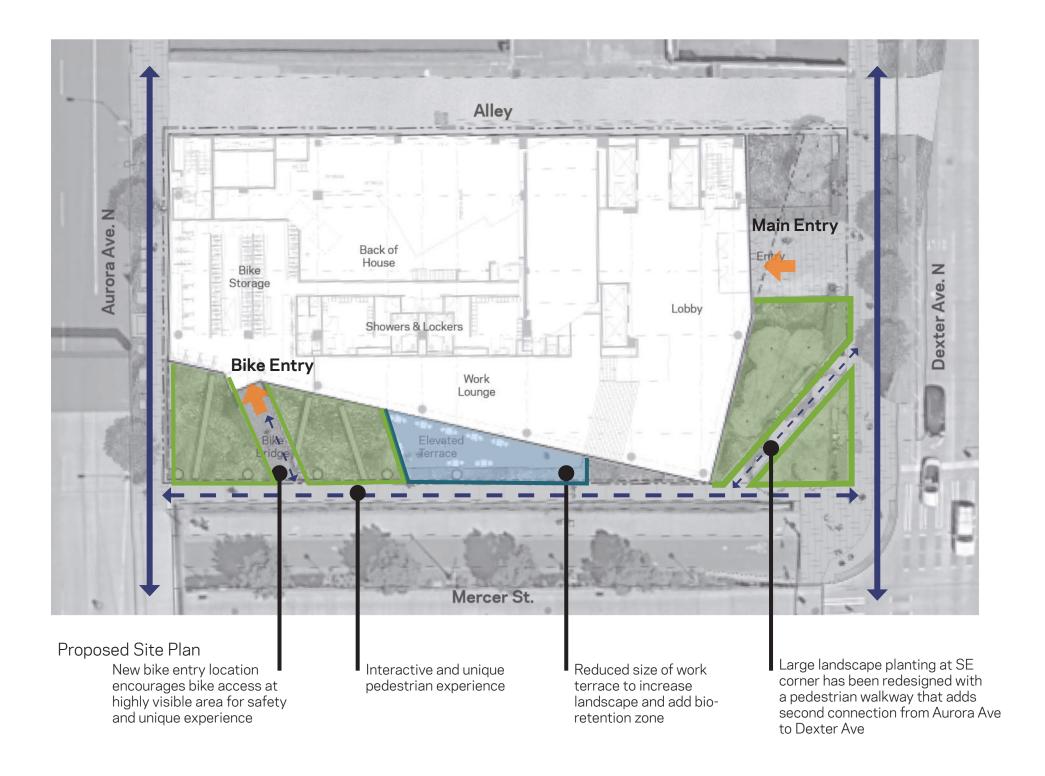
RELEVANT DESIGN GUIDELINES: CS2-D-2. Existing Site Features, CS2-B Adjacent Sites, Streets, and Open Spaces, South Lake Union CS2-4-a. All Corner Sites, South Lake Union CS1-3 Topography and Elevation Changes. CS2-B-1. Site Characteristics, CS2-B-2. Connection to the Street, PL1-B-1. Pedestrian Infrastructure, South Lake Union PL2-2 Walkways and Pedestrian Interest.



RESPONSE

2. Ground Plane

- **a.** The approved massing by the Board has been retained and enhanced at the ground plane by:
- 1) Moving the bike entry from the Alley to Mercer to encourage a better experience through better activation, use, and safety
- 2) Creating an active landscape to the south of the project. An elevated terrace sits adjacent to a work lounge, which will be used throughout the day by scientists as a meeting and collaboration space. This space can support visitors from other science buildings in the area.
- 3) Opening the work lounge to the lobby. The two programmed elements meet at a series of steps that serve as a presentation space at the corner of Dexter and Mercer.
- **b.** To improve the circulation between Dexter and Aurora along Mercer, the sidewalk has been redesigned as a gradual ramp leading to stairs at Aurora. Removing stairs that previously existed near Dexter allows this ramp to serve pedestrians and cyclists. As one moves along this edge, the space beneath the building grows by 40' to promote an open experience elevated above Mercer.



2. Ground Plane

c. The Board discussed the corner of Dexter Ave and Mercer Street, commenting that though they appreciated the generous amount of landscaping, they questioned if there could be further engagement with the public. This is a very busy corner, so perhaps more occupiable space would be more appropriate. However, no specific solution was recommended. The Board requested studies be provided at the next meeting, illustrating the refinement of the corner with the goal of improving connectivity, circulation, and public engagement. The Board requested the following information and design responses.

i. Analysis on how the site is used currently and more clarity on how the design team envisions this site functioning moving forward.

RELEVANT DESIGN GUIDELINES: South Lake Union CS2-3-a. Aurora and Dexter Ave N, CS2-B-2. Connection to the Street, South Lake Union CS1-C Topography, PL4-B Planning Ahead for Bicyclists, South Lake Union DC4-2-e. Sense of Place, PL4-A-1. Serving All Modes of Travel

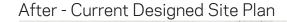
RESPONSE

2. Ground Plane

c. During EDG, the board's discussion of the intersection of Dexter and Mercer considered the balance between framing of the intersection with planting as proposed to mitigate the overall extent of paved area and the potential for greater pedestrian engagement by enlarging the paved area. The revised design enhances the potential for public engagement at the corner of Dexter and Mercer by adding a layer of seating and pedestrian passage into the sloping site along with opening up the corner. The new pedestrian pathway and seating frames the public ROW where a grove of Alders with native planting below is supported by SDOT as a sustainability feature in this unique urban condition. The additional layer of planting and trees within the SDOT property maintains accessible slopes through the intersection and frames the intersection. The revised design shifts the orientation of the planting edge to widen the sidewalk between Dexter and Mercer and more equally frame the intersection to allow one of the feature benches to better serve pedestrians crossing either street.

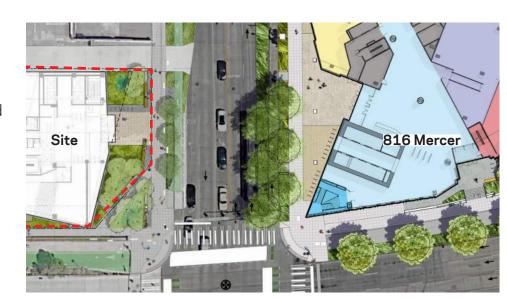
i. See circulation diagrams with before and after







feature site elements with integrated lighting and public seating (scaling elements)



overall intersection scale

2. Ground Plane

- c. ii. Continue to evolve the ground plane and gateway concept and the way people (pedestrians, cyclist, automobiles) move through and around the project.
- iii. Consider how topography can inform decision making regarding landscaping and site circulation refinements.
- d. Bicycle Circulation. The Board would like more information on how bike access and safety has been refined to address the conflicts at the alley improve bike access.

RELEVANT DESIGN GUIDELINES: CS2-D-2. Existing Site Features, CS2-B Adjacent Sites, Streets, and Open Spaces, South Lake Union CS2-4-a. All Corner Sites, South Lake Union CS1-3 Topography and Elevation Changes. CS2-B-1. Site Characteristics, CS2-B-2. Connection to the Street, PL1-B-1. Pedestrian Infrastructure, South Lake Union PL2-2 Walkways and Pedestrian Interest.

RESPONSE

2. Ground Plane

- cii. The revised Mercer sidewalk grading to incorporate a sloped ramp allowing for the removal of the existing stairs. A cut-through path at the south east corner provides direct access to the Upper Mercer sidewalk while maintaining a vegetated buffer for pedestrians at the intersection
- ciii. The Wedge planting utilizes falling natural grade from Aurora to Dexter to enhance the cascading bio-retention zones. As one cell fills up it can then cascade into the adjacent cell, reminiscent of the natural water hydrology.
- d. Access to bike parking has been moved to the cut along Mercer, creating more activity at the cut and an enhanced cycling experience. A bridge over bio-retention connects bikers to a transparent parking area





Current Designed Site Plan

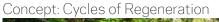


follows the natural grade cascade down Mercer St

2. Ground Plane

Clarify if the intent is for the landscaping to be a counterpoint to the strong angles of the massing, or for those angles to continue down to the ground plane. Perhaps a softer ground plane could also help mitigate design concerns.

RELEVANT DESIGN GUIDELINES: South Lake Union CS2-3-a. Aurora and Dexter Ave N, CS2-B-2. Connection to the Street, South Lake Union CS1-C Topography, PL4-B Planning Ahead for Bicyclists, South Lake Union DC4-2-e. Sense of Place, PL4-A-1. Serving All Modes of Travel









ecological regeneration



RESPONSE

2. Ground Plane

The soft, lush planting of the Landscape is imagined to provide a contrast to the strong angles of the building massing. This contrast allows the Landscape to feel larger and 'continuous' rather than fragmented and piecemeal.

Pathways into the building cut, slice, and bridge the planting, helping to reinforce the idea of a larger continuous landscape.

'Log' seating and retaining elements are envisioned to recall the logging history of the South Lake Union area and explicitly contrast with building angles.



3. Materials and Facade Development

- a. The Board emphasized that as the project evolves with material detailing, they expect to see more than a glass box with fins and recommended reviewing the tall building design guidelines. The Board expects to see thoughtful material application at this gateway corner which includes detailing of secondary architectural elements and depth to provide greater visual interest and character, and further breakdown of the scale of the building.
- **b.** The Board noted consideration of the site's context including proximity to Lake Union may help to inform the material palette and facade development.

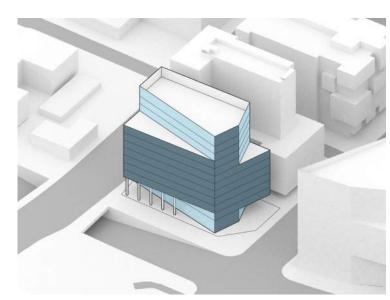
RELEVANT DESIGN GUIDELINES: South Lake Union CS2-3-a. Aurora and Dexter Ave N. CS2-B-2. Connection to the Street. South Lake Union CS1-C Topography, PL4-B Planning Ahead for Bicyclists, South Lake Union DC4-2-e. Sense of Place, PL4-A-1. Serving All Modes of Travel

RESPONSE

3. Materials and Facade Development

- a. Dynamic massing moves have been created to carve through the glass box, establishing a series of small and mid-level scales that further break down the massing as seen at EDG. These moves visually connect the outdoor terrace and frame a notched balcony at the corner of Aurora and Mercer. The northeast and northwest corners are accented by stairs that provide interest, movement, and a unique expression.
- **b.** Referencing the industrial history of South Lake Union, wood, metal and glass reflect materials that have long been a part of the area for the last century. These familiar materials are highlighted by planting and program; accents occur throughout the project's landscape, work lounge, exterior balconies, stairs, and terraces.

EDG-PREFERRED MASSING CONCEPT



Ariel View from SE Corner

DRB- MATERIAL DEVELOPMENT



Street View from SE Corner



Enlarged South Notch



Enlarged East Balconies



The timber industry and shipping industry in South Lake Union provide a rich history in South Lake Union. The wood not only provides warmth, but aims to address the contextual history of the Timber industry.



metal

South Lake Union has always been filled with innovation. Henry Ford built his first model T factory and William Boeing began manufacturing seaplanes in South Lake Union. As the neighborhood continues to innovate in the science industry, the metal aims to frame and highlight the science story and build upon it.



glass 1



glass 2

The glass and frit optimize natural daylighting. The frit creates solar protection without obstructing views of modern day South lake Union and Down Town Seattle

PRESENT:

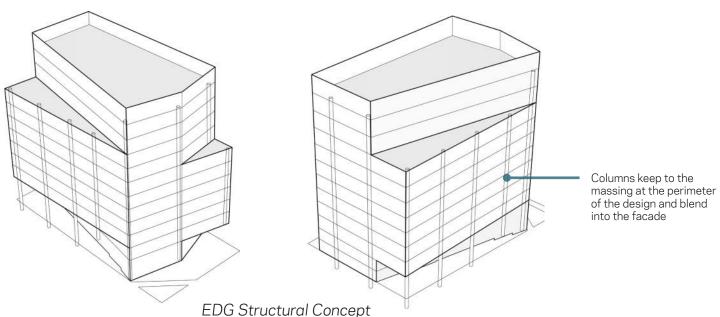
2. Ground Plane

c v. Clarify how the columns are integrated into the overall concept and detailed.

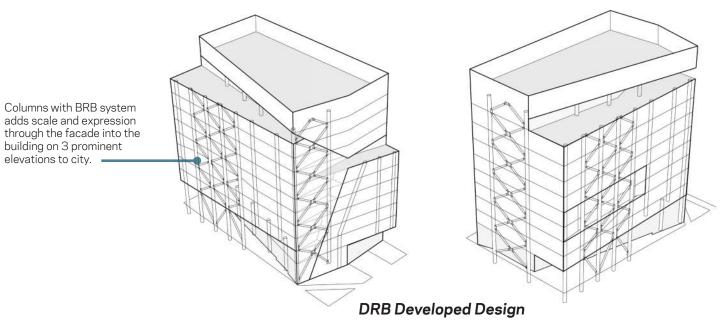
RELEVANT DESIGN GUIDELINES: CS2-D-2. Existing Site Features, CS2-B Adjacent Sites, Streets, and Open Spaces, South Lake Union CS2-4-a. All Corner Sites, South Lake Union CS1-3 Topography and Elevation Changes. CS2-B-1. Site Characteristics, CS2-B-2. Connection to the Street, PL1-B-1. Pedestrian Infrastructure, South Lake Union PL2-2 Walkways and Pedestrian Interest.









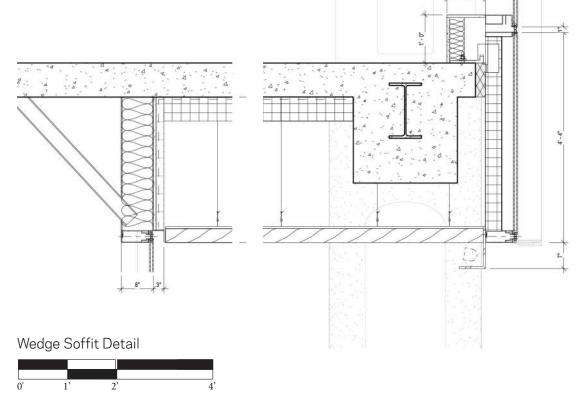


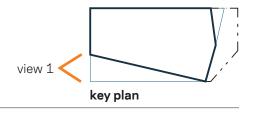
RESPONSE

2. Ground Plane

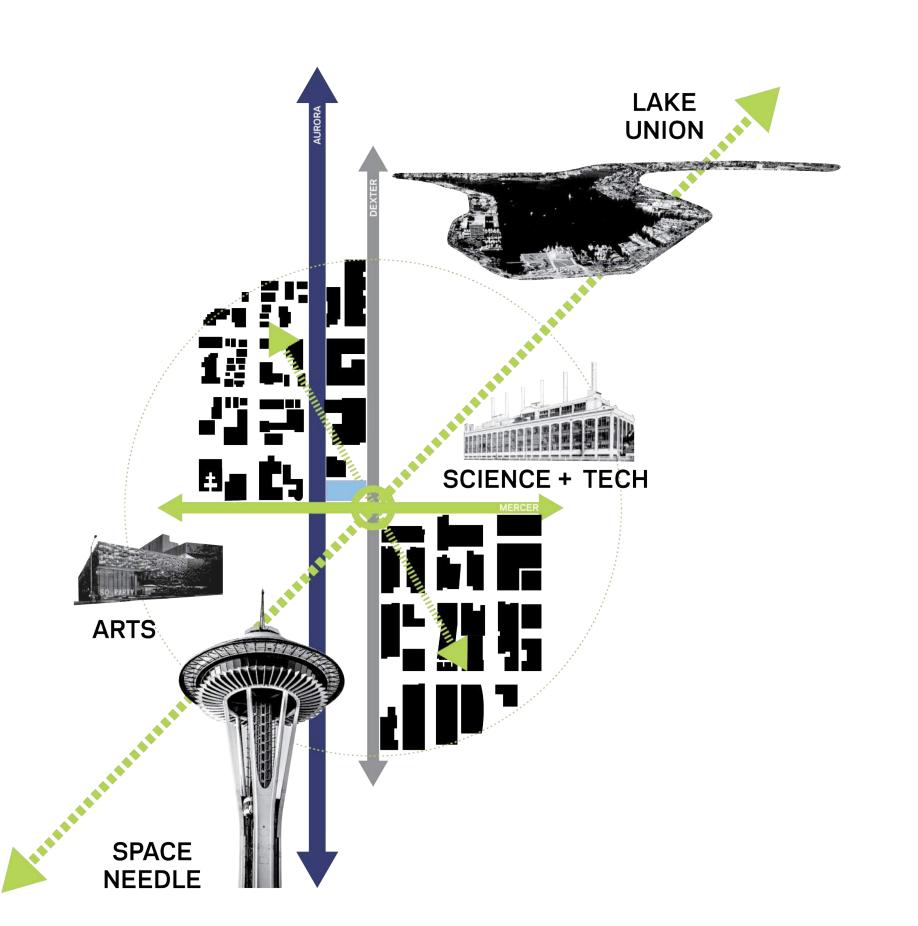
c v. The column and BRB system sit within the bio-retention area to provide a neighborhood scale and tangible expression of systems. The sealed concrete reflects the rawness of an industrial material, while the painted steel plate connections will express engineering finesse. A wood soffit complements the greenery below the overhang, allowing the columns to gently disappear overhead. Lighting will enhance the exterior elements in the evening.







CONCEPT DEVELOPMENT 03





SIGNIFYING AN IMPORTANT GATEWAY

Playing a major role in denoting the uniqueness of scale, history, and program occurring in South Lake Union, 601 Dexter announces this context through its massing, materials, and high visibility to science.

SCIENCE IN SEATTLE

CITY CONTEXT

Located on the edge of South Lake Union, the site marks the intersection between Lake Union, Seattle Center, and Denny Park. The project connects adjacent neighborhoods through massing and materials while establishing a strong architectural identity that emphasizes the interactions of science and research.





SCIENCE IN SOUTH LAKE UNION

9-BLOCK CONTEXT

The 9-block context around 601 Dexter has undergone substantial change over the last decade, and it will continue to evolve over the decade to come. Historically, sites to the north and west have housed residential and retail buildings that are low and mid-rise heights. To the south and east, a mix of office, lab, and residential towers have begun to shape the South Lake Union experience.

Recent developments have created a zone of research, of which 601 Dexter will play a prominent role in not only as a central contributor in science in but also as a threshold to a diverse SLU.



SCIENCE IN SOUTH LAKE UNION







700 Dexter Ave N SDCI #3026942 14-Story 2-Tower Office w/ Retail



850 Republican St Multi-story Lab / Research building



800 Mercer Block SDCI #3035865-LU 12-Story Lab / Office

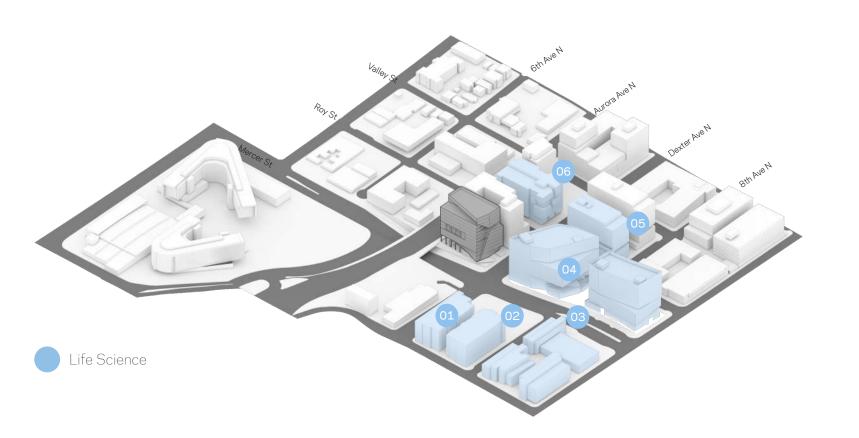


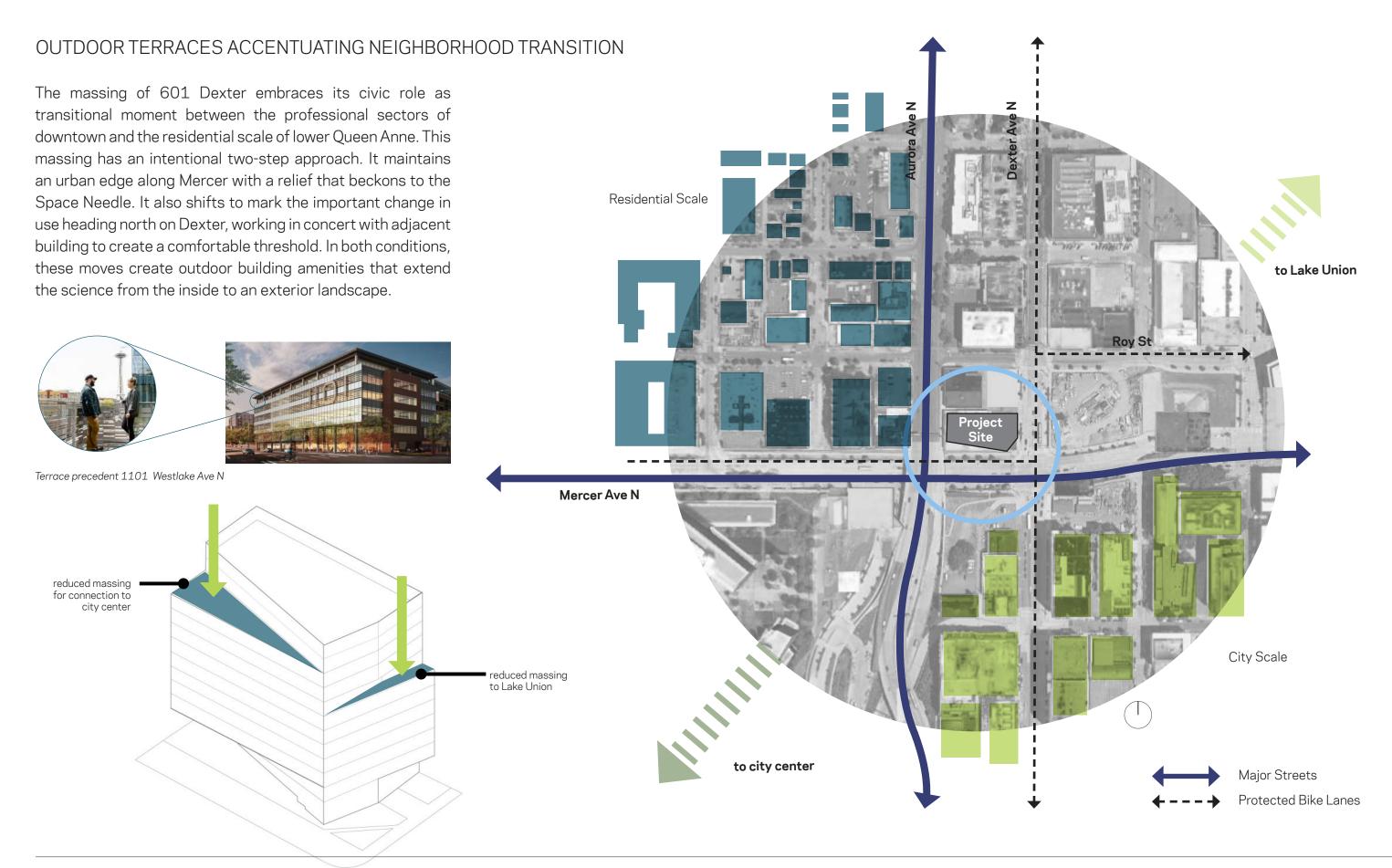
700 Dexter Ave N SDCI #3026942 14-Story 2-Tower Office / Lab w/ Retail



701 Dexter Ave N SDCI #3026844 12-Story Lab / Office

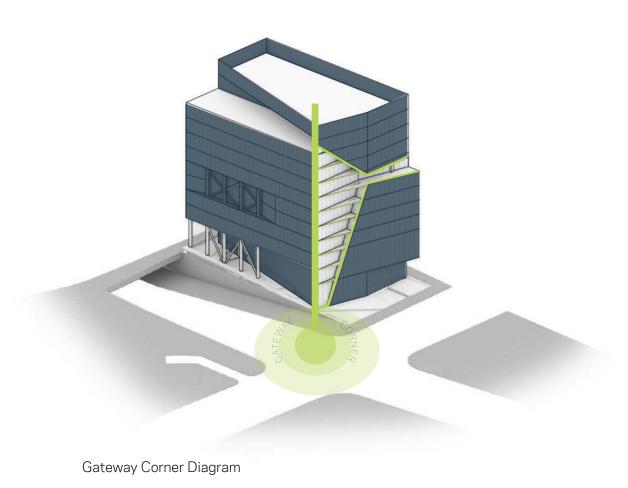
The language of buildings adjacent to Mercer and Dexter is varied in articulation and expression. From glassy facades to solid elements, each building works to house the science that happens within. However, few of the projects provide the public with strong connection to the activities of research happening within those walls. 601 Dexter looks to invite the public to various modes of scientific exploration through multiple means of research interaction.

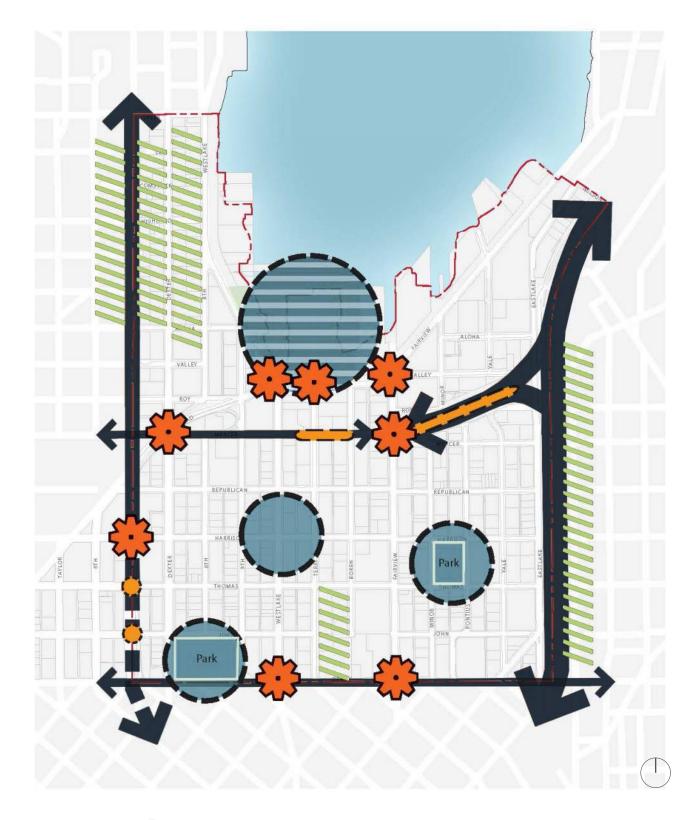




A GATEWAY CORNER REVEALING THE COLLABORATIVE PROCESS

Solidifying the intent of the South Lake Union Design Guidelines, 601 Dexter anchors the corner of Mercer and Dexter with a series of balconies that establish a social intersection for scientists. Located near stairs, passenger elevators, and the laboratory, this condition ensures that this important pivot point will be activated by research.





LEGEND

Neighborhood Heart



Regional Heart



Gateway

Processional Gateway



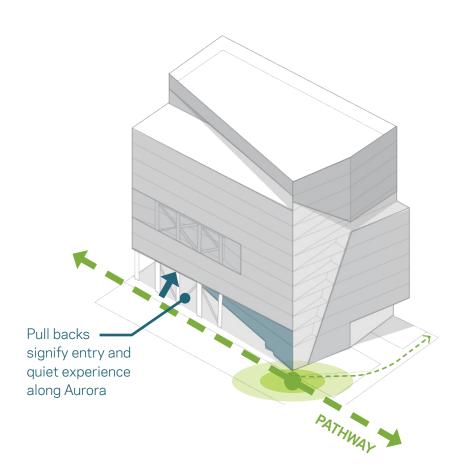
Infrastructure barrier

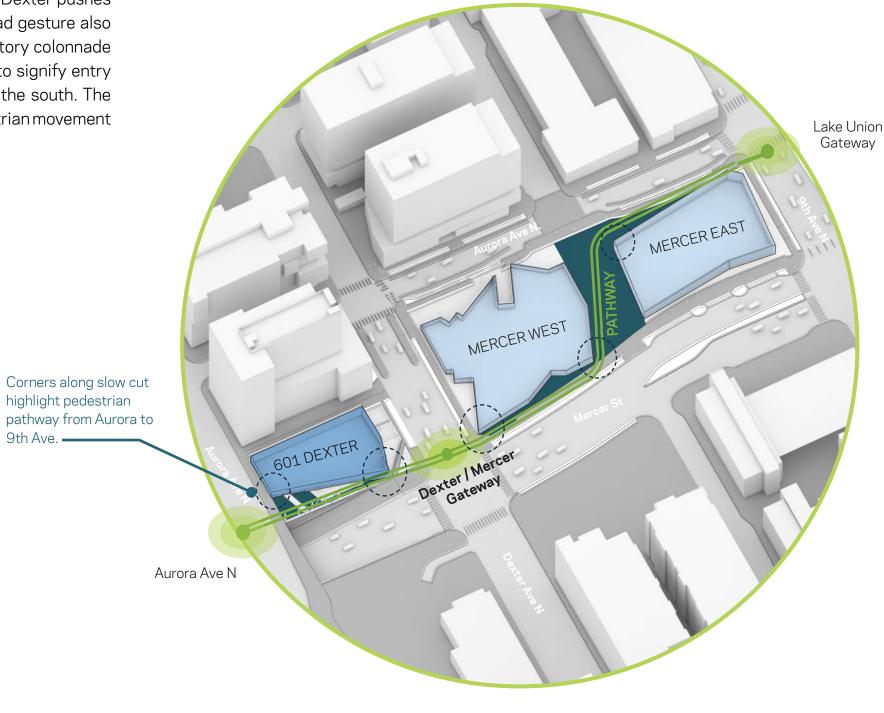


Challenging topography (Public & Private Hill climbs encouraged)

A TRANSPARENT GROUND PLANE EXTENDING THE URBAN ENVIRONMENT

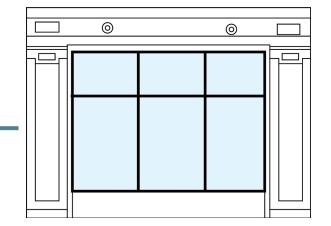
Sharing a gateway corner with the Mercer Blocks, 601 Dexter draws upon that site's intent to both open and energize the ground plane through pedestrian invitation. 601 Dexter pushes inward from the Gateway corner to create an inviting path to Aurora; the broad gesture also welcomes visitors arriving from the west into South Lake Union along a two story colonnade offering depth and scale along Mercer. The building pulls back along Dexter to signify entry and create a quiet experience for pedestrians away from the busy traffic to the south. The transitional experience from Aurora to Mercer to Dexter reflects exterior pedestrian movement through uses that promote interaction between building occupants.





FACADE AS A MEANS OF DISPLAY

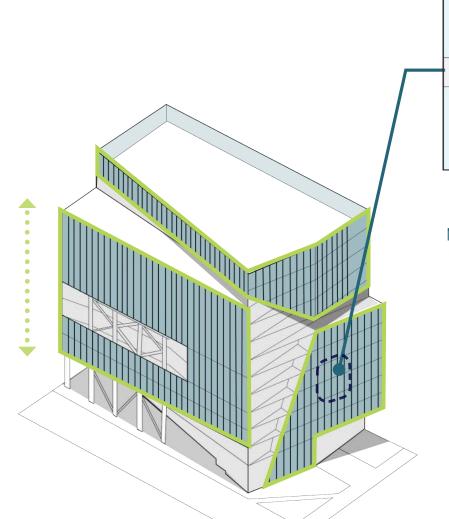
South Lake Union may be known for its industrial history, but many of the respected buildings from its past were quite successful in displaying the interior activities and products to passers-by. Leveraging their characteristics of material, proportion and verticality, 601 Dexter emulates these storefronts to provide an invitation to science through transparency and display.



Ford Manufacturing Storefront Elevation Diagram

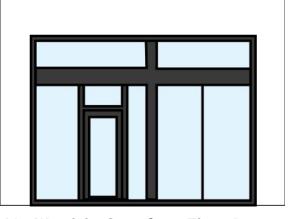


Ford Manufacturing Storefont Picture

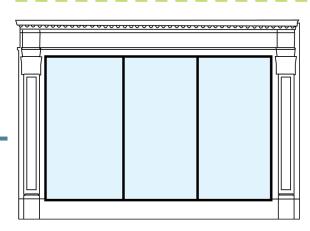


601 Dexter Storefront Facade

Material transparency at "storefront" facade connects public into the building



425 Westlake Storefront Elevation Diagram



William O McKay Ford and Pacific Lincoln Mercury Auto Storefront Elevation Diagram



425 Westlake Storefront Picture

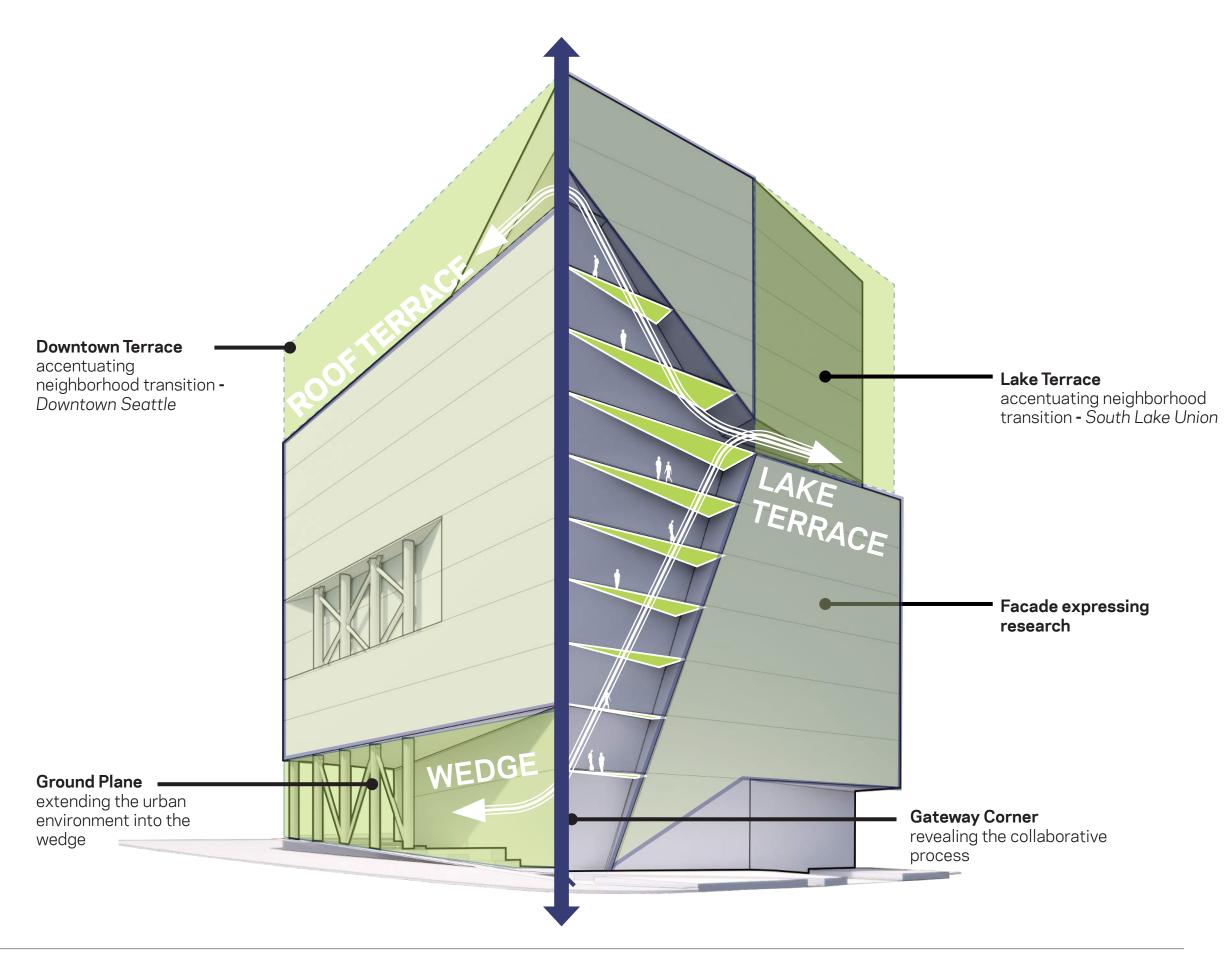


William O McKay Ford and Pacific Lincoln Mercury Auto Storefront Picture

DEVELOPMENT SUMMARY

A transitional moment between downtown workplaces and residential neighborhoods, the 601 Dexter site also plays a pivotal role in anchoring Seattle's commitment to science along the heavily traveled thoroughfare of Mercer Avenue.

Denoted as a Gateway Corner by South Lake Union Design Guidelines, this site has an opportunity to express scientific programs in ways that are rarely seen by the public. This inviting mindset creates an active "science in the city" environment that clearly participates across streets and along every pedestrian edge.

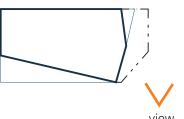


CONCEPTREALIZATION ()4

GATEWAY CORNER

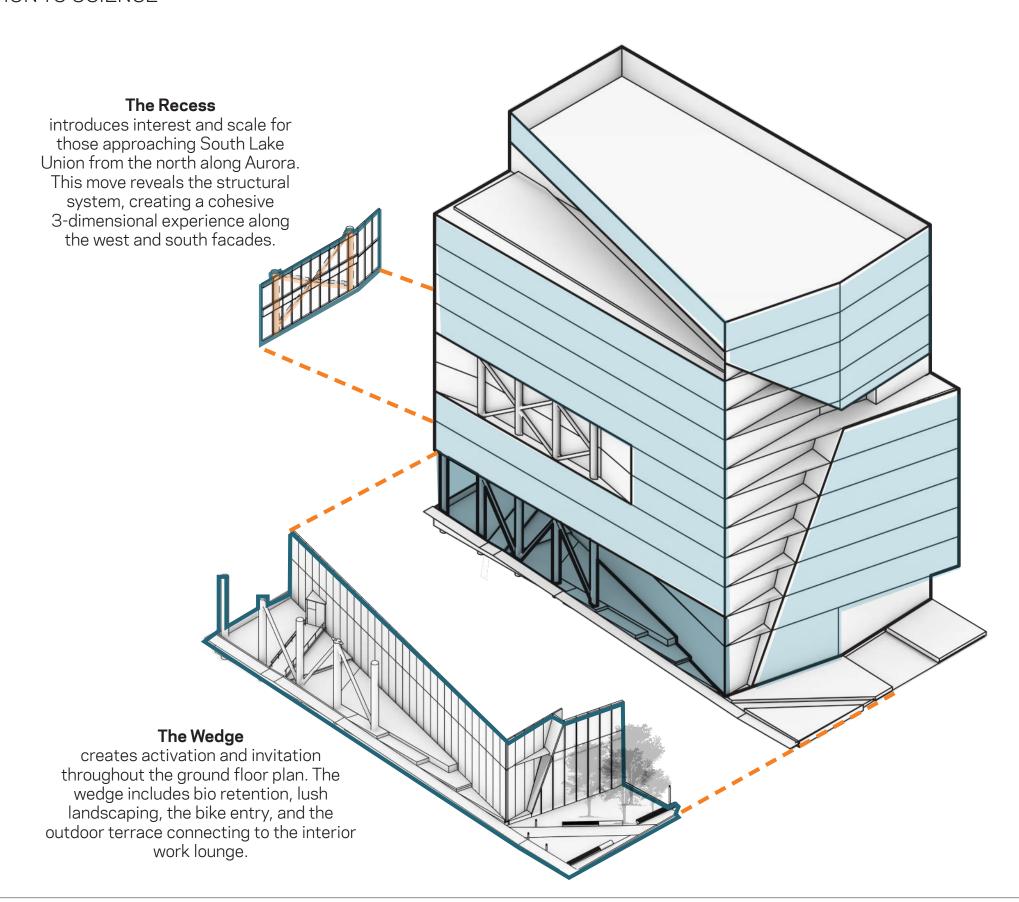


key plan



GROUND PLANE

INTRODUCTION TO SCIENCE



GROUND PLANE EXPRESSION



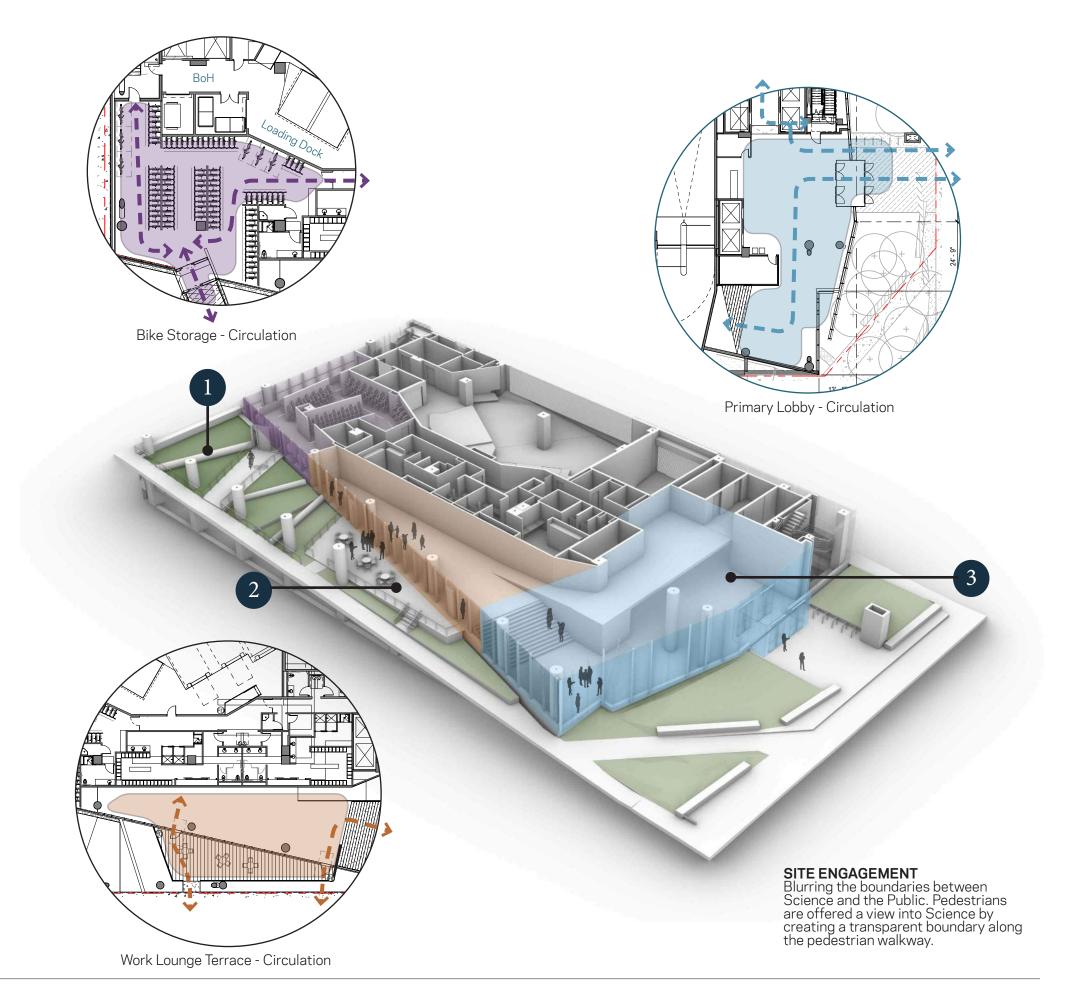
1 BIKE STORAGE



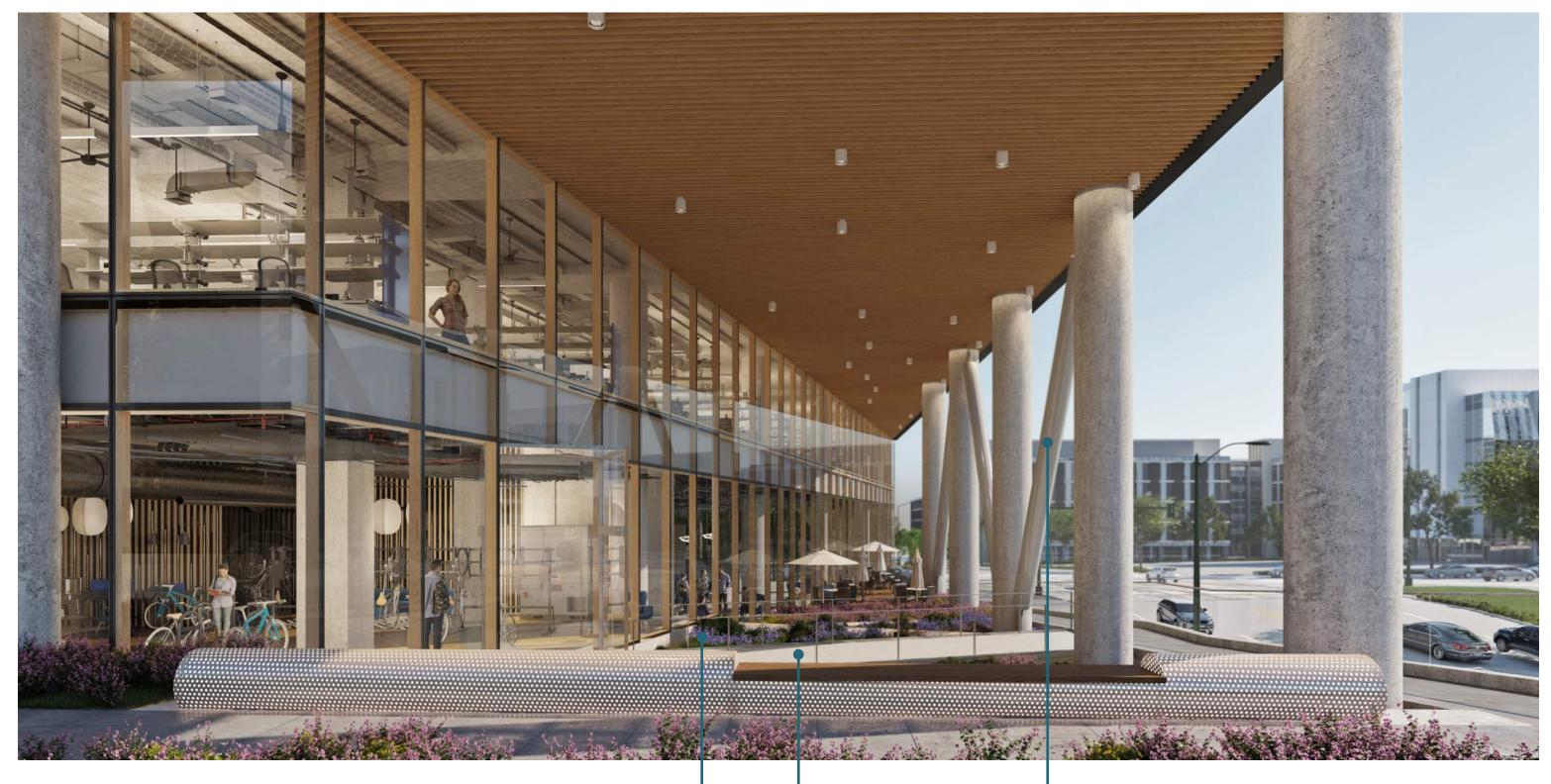
WORK LOUNGE TERRACE

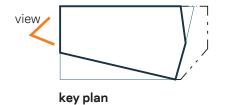


3 PRIMARY LOBBY



BIO-RETENTION BIKE BRIDGE





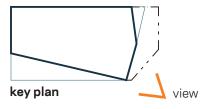
BIO-RETENTION
Lush landscape provides a visual softness between the site and street scape

BIKE ENGAGEMENT
Designated pathway that
crosses over the bio retention

STRUCTURAL EXPRESSION
Large structural members engage
the landscape and offer a unique
experience for the bikers as they cross
under the frames to access the bike
bridge

DEXTER - MERCER CORNER



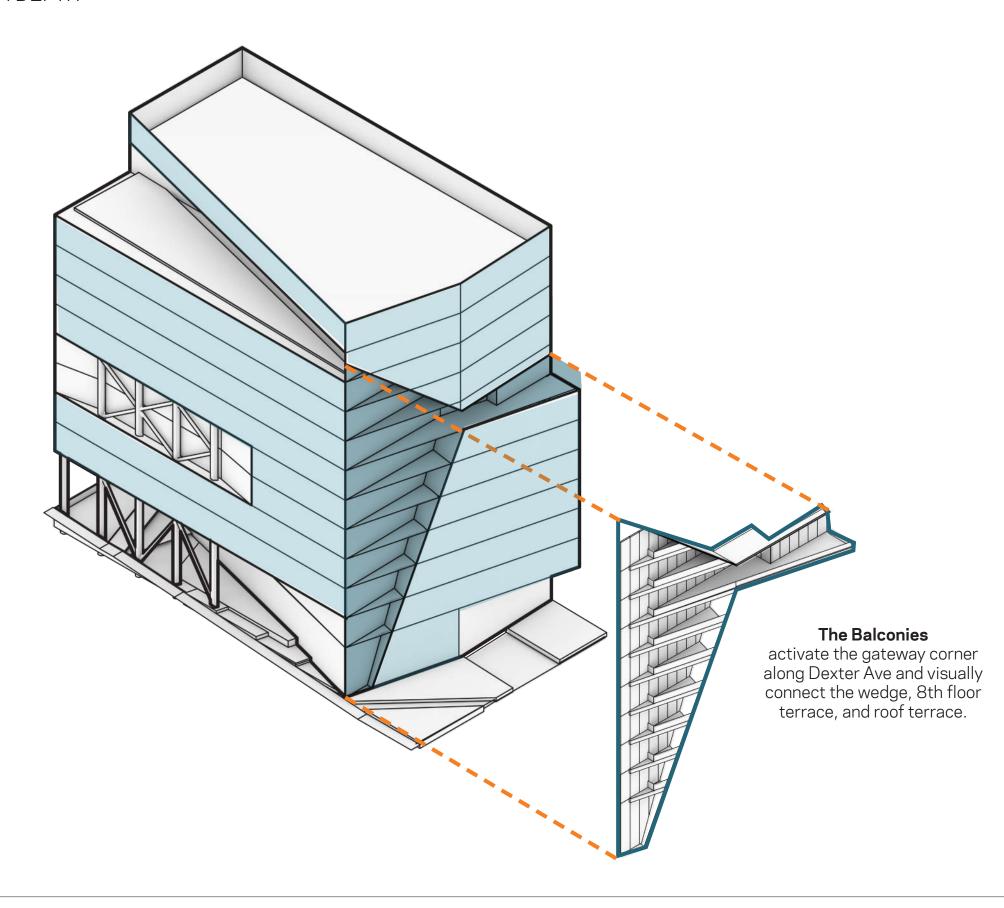


STRUCTURAL EXPRESSION
Large structural members engage
pedestrians and users walking
through the site

SITE ENGAGEMENT
Blurring the boundaries between
Science and the Public. Pedestrians
are offered a view into Science by
creating a transparent boundary along
the pedestrian walkway.

INVITATION CORNER

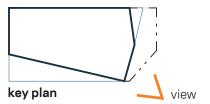
ACTIVE COLLABORATION THROUGH DEPTH



DEXTER - MERCER GATEWAY CORNER

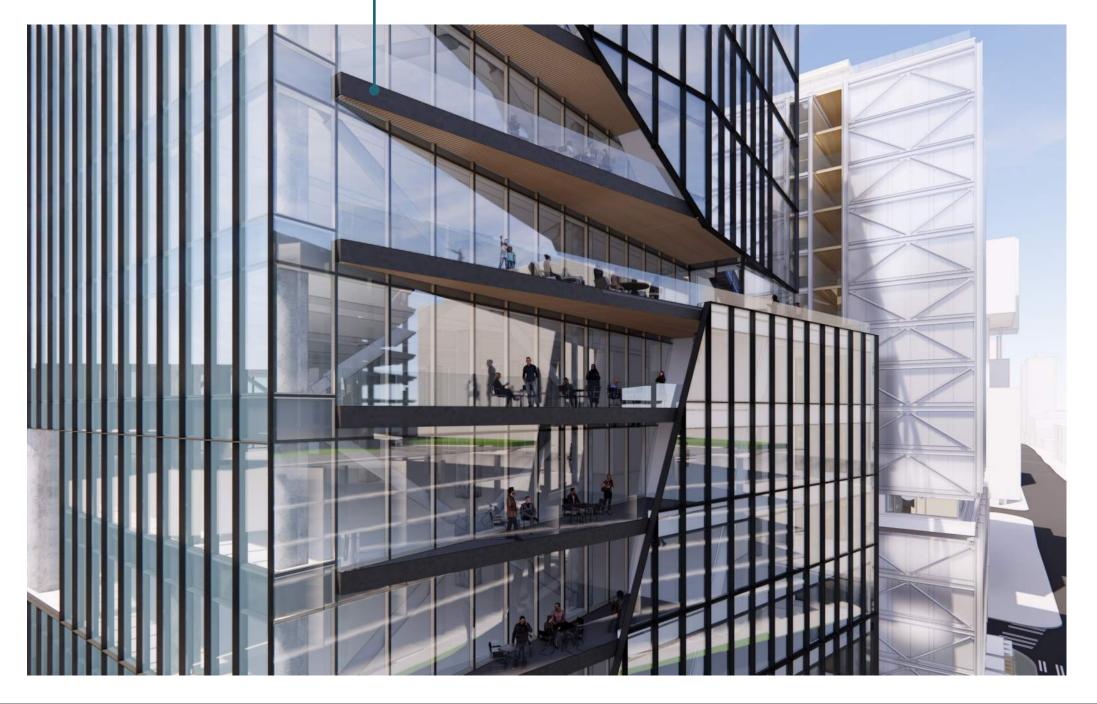


View from Mercer St SE corner



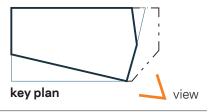
DEXTER - MERCER GATEWAY CORNER

ENGAGEMENT
The balconies create an outdoor amenity that breaks down the massing along Dexter by softening the angularity of the geometry



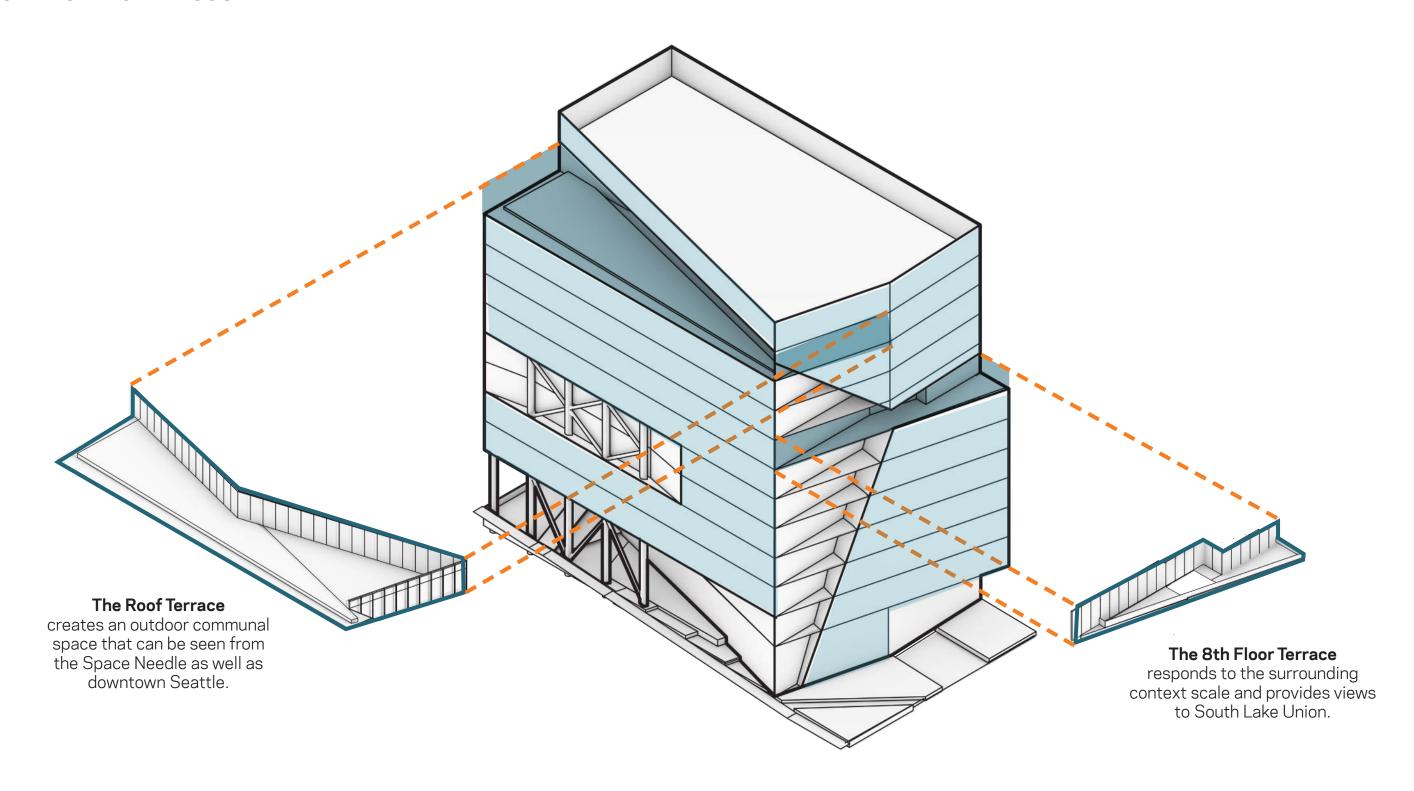


Option with Balcony Stairs



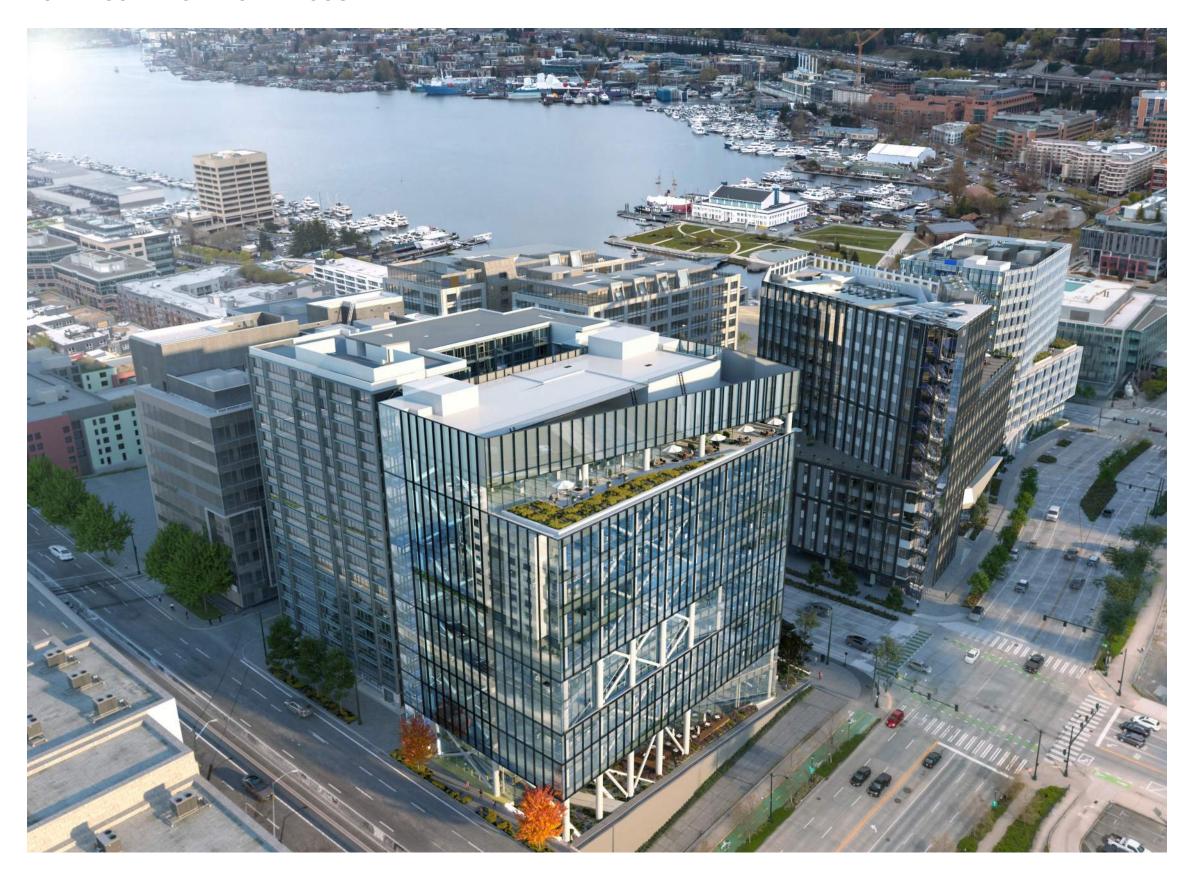
OUTDOOR TERRACES

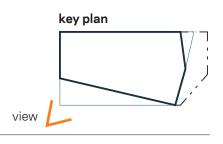
ACTIVE COLLABORATION THROUGH DEPTH



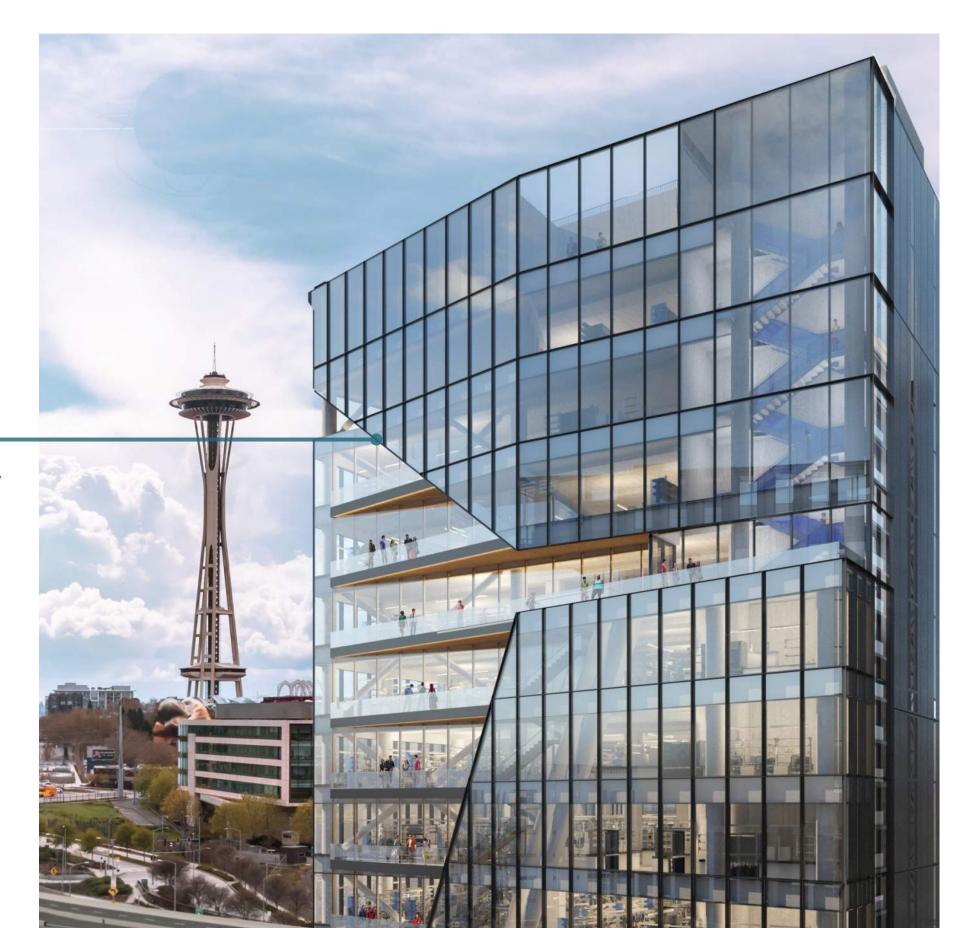
OUTDOOR TERRACES

ACTIVE COLLABORATION THROUGH DEPTH



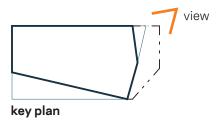


LAKE TERRACE



CONNECTION

An angled facade visually connects outdoor amenities by linking terraces and balconies

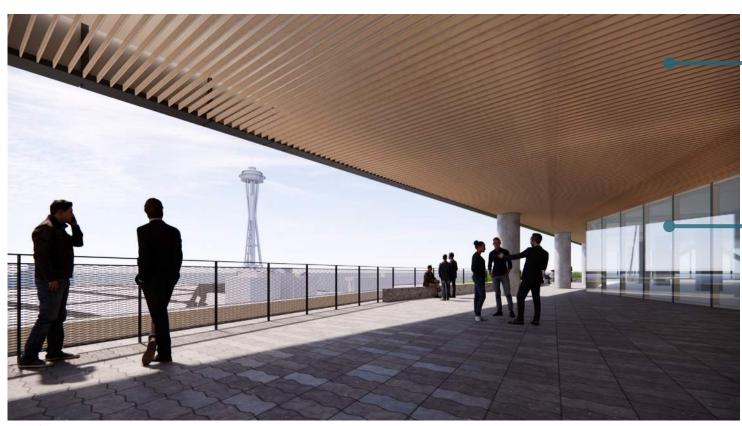


DOWNTOWN TERRACE



CONNECTION

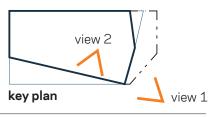
An angled facade visually connects outdoor amenities by linking terraces and balconies



CONNECTED MATERIAL

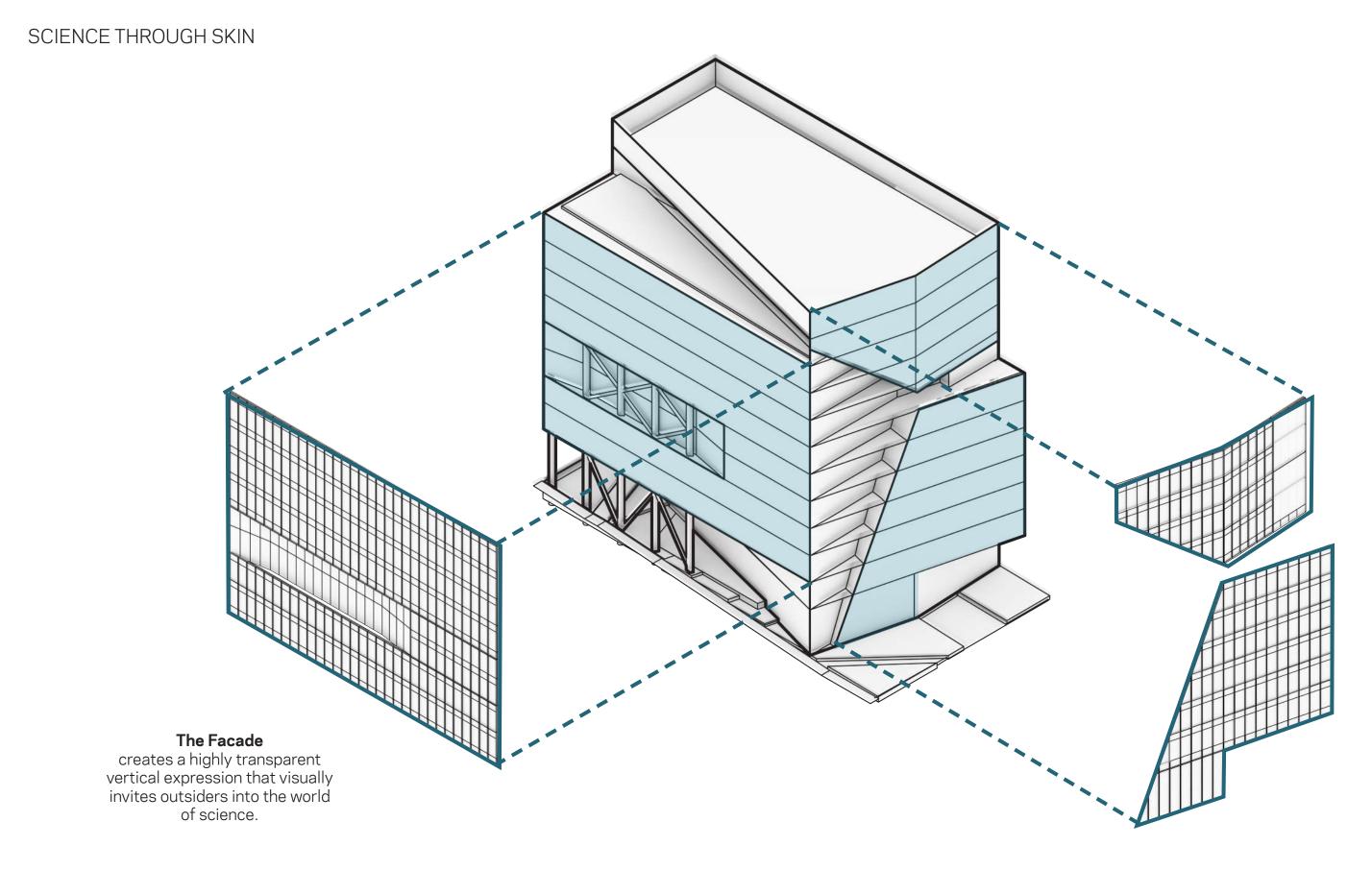
INVITATION

Building massing pulls back from corner to create invitation and covered outdoor space



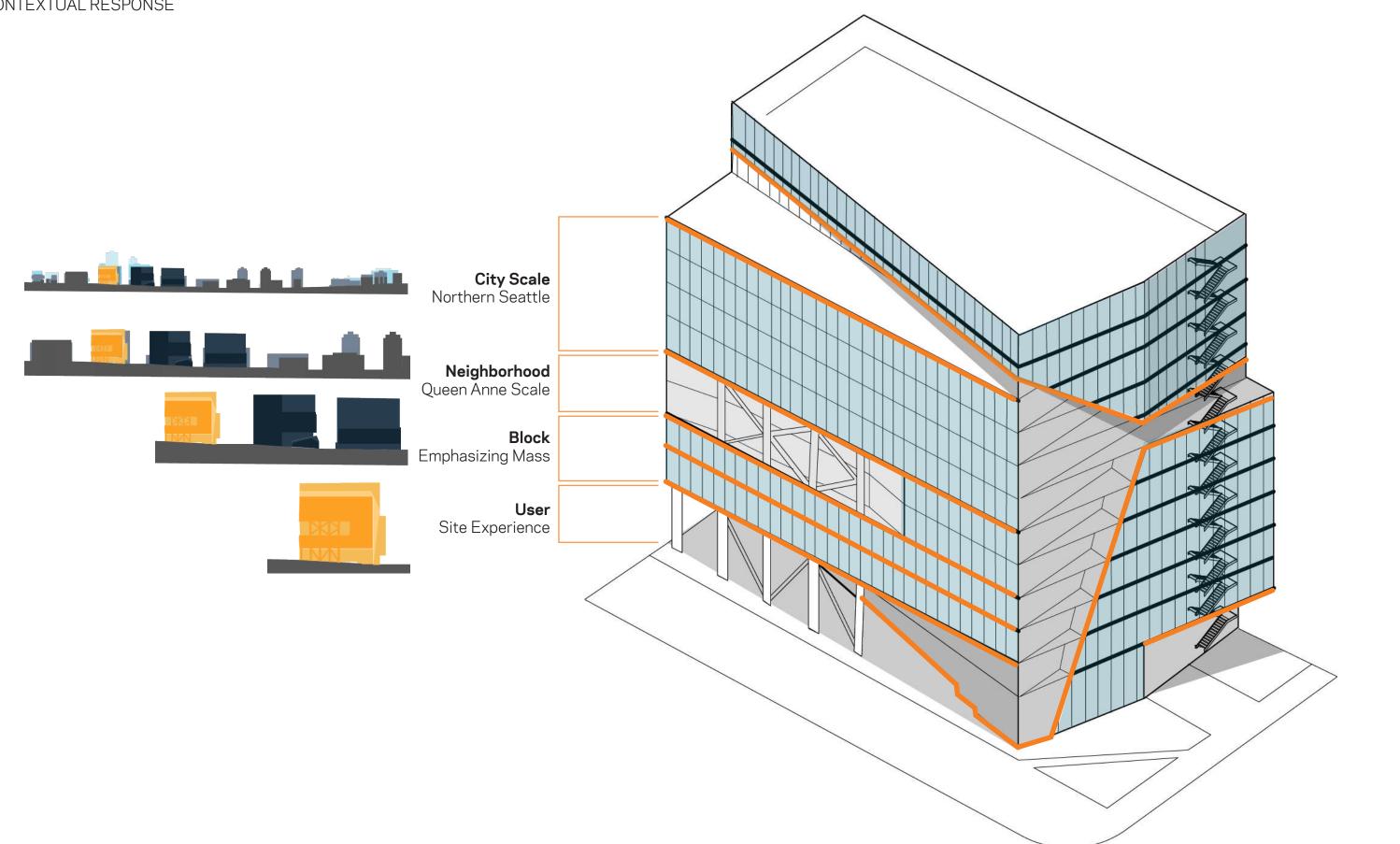
view 2

FACADE ON DISPLAY



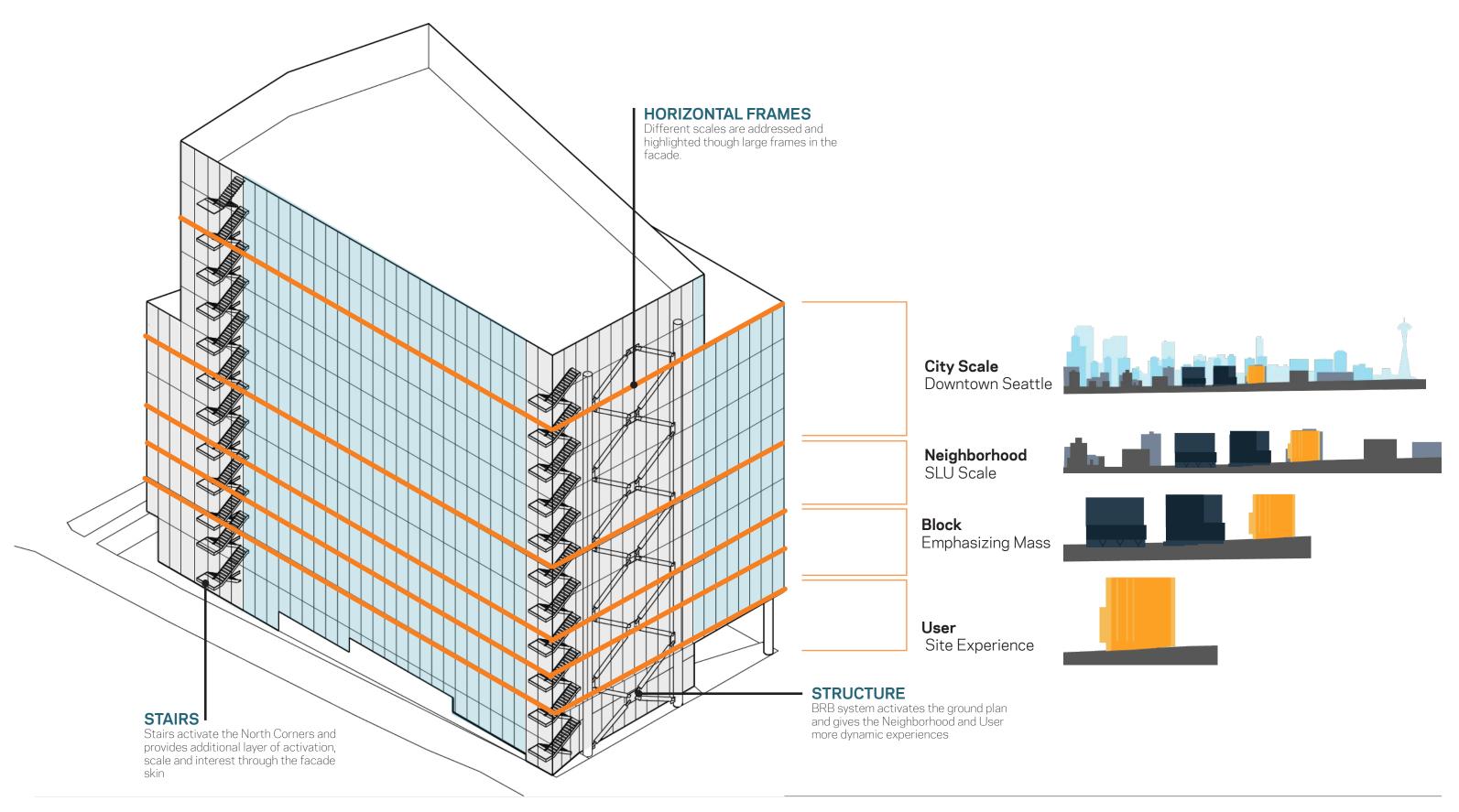
FACADE STORY

CONTEXTUAL RESPONSE



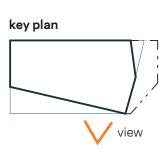
FACADE STORY

CONTEXTUAL RESPONSE



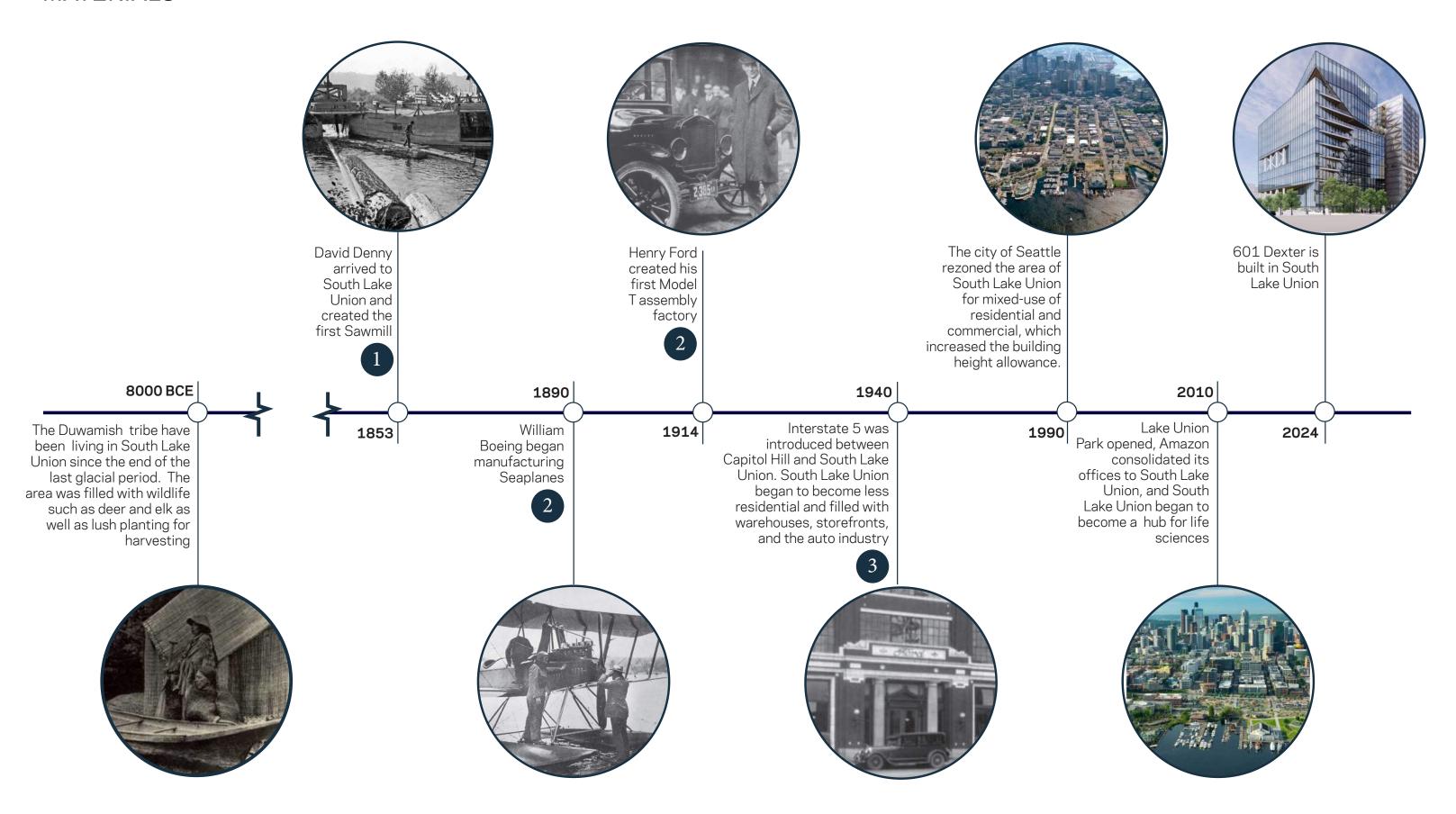






PEDESTRIAN EXPERIENCE ()5

MATERIALS



MATERIALS

PAST: The South Lake Union has a rich history of materials and innovation. Beginning in 1853, David Denny arrived in South Lake to create the first sawmill in Seattle drawing in additional employees and business. Denny expanded upon his timber business to create a coal industry in Seattle, which lead to creating transportation routes and infrastructure that is still utilized in Seattle today. around the turn of the century, other streams of revenue in South Lake Union began to boom leading to industrial, commercial, and cultural innovation. William Boeing began manufacturing seaplanes around South Lake Union, Henry Ford built his model T Factory, and much more.

PRESENT: South lake Union is becoming a hub for life science that strives for innovation. 601 Dexter aims to be innovative while also respecting and reflecting the rich history of South lake Union.

The wood, concrete, and metal frames of 601 Dexter are raw, honest, and authentic to reflect the industrial past of South Lake Union. The glass creates a highly transparent facade stretched vertically to place science on display, as an innovative way to represent the historical storefronts of South Lake Union.

HISTORY OF SOUTH LAKE UNION

Reflecting the Past through Raw and Honest Materials







SOFFITS AND GLULAM BACKING Honesty in Materials



METAL WALL PANEL Industrial Innovation



METAL FRAMES, SHADOW BOX, AND WALL PANEL Industrial Innovation



3 CONCRETE COLUMNS Honesty in Materials



3a CONCRETE COLUMNS AND PAVERS Honesty in Materials

SCIENCE ON DISPLAY Highly Transparent Facade



VISION GLASS 1
Science on display

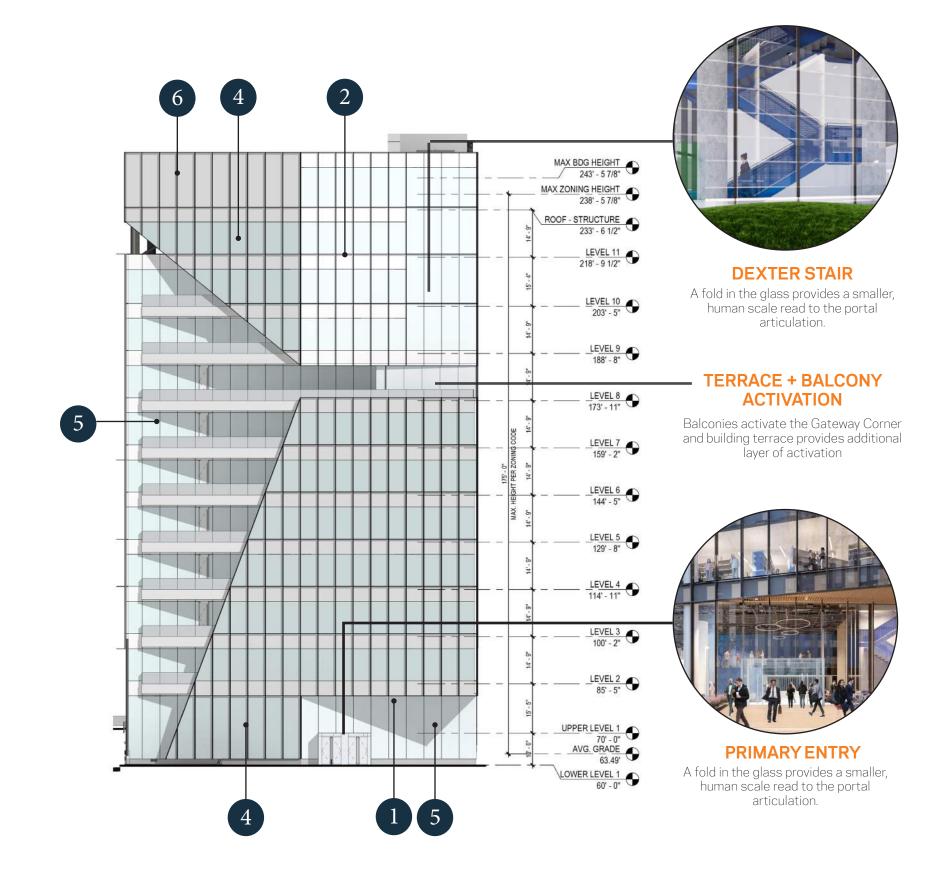


VISION GLASS 2 Science on Display



6 SHADOW BOX Dimension in the Facade

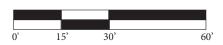
DEXTER AVE. - ELEVATION



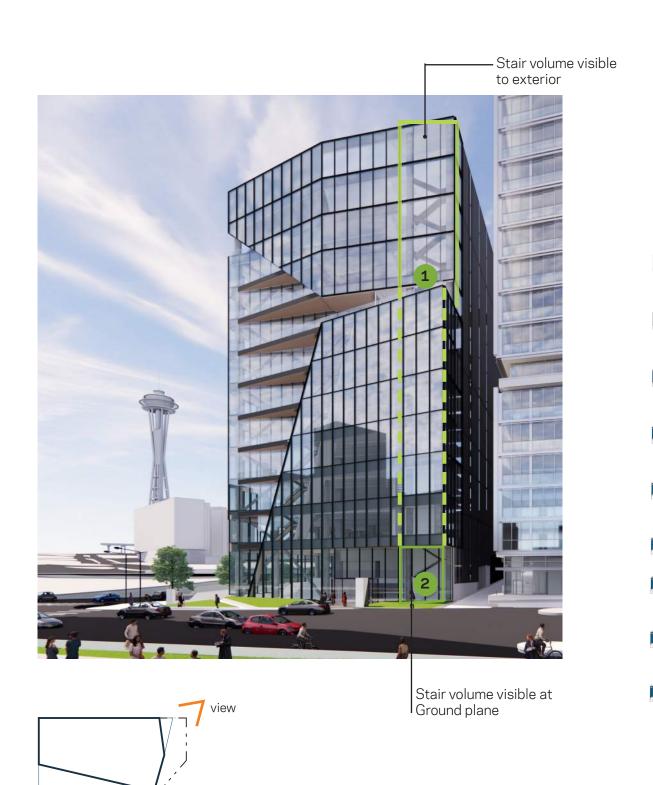
MATERIAL KEY

- Soffit wood baffle
- French gray metal frames Concrete
- Vision Glass 1
- Vision Glass 2
- Shadow Box

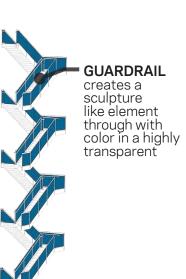
EAST ELEVATION



DEXTER AVE. - STAIR EXPERIENCE

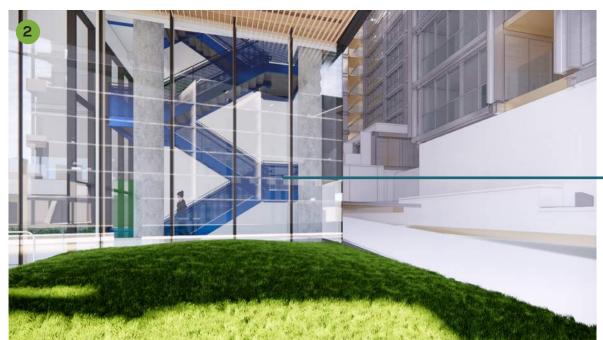


key plan



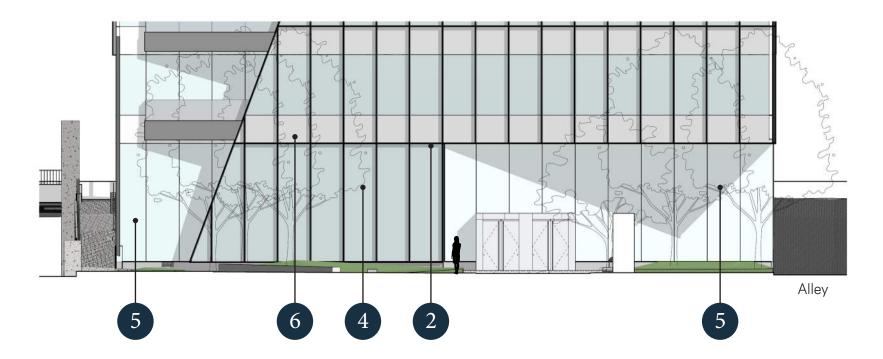


ENGAGEMENT
Stair serves as a sculptural element with views through the terrace and ground floor



Colorful guardrail through the transparent facade encourages the usage of the stairs.

DEXTER AVE. - EXPERIENCE



MATERIAL KEY

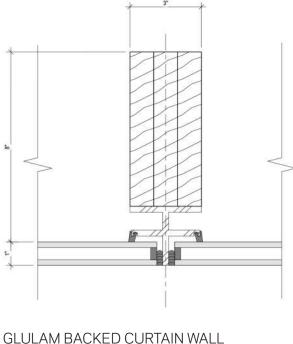
- Soffit wood baffle French gray metal frames Concrete

- Vision Glass 1 Vision Glass 2
- Shadow Box

ENLARGED EAST ELEVATION











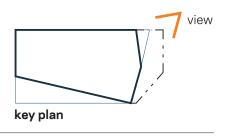
DEXTER AVE.- EXPERIENCE



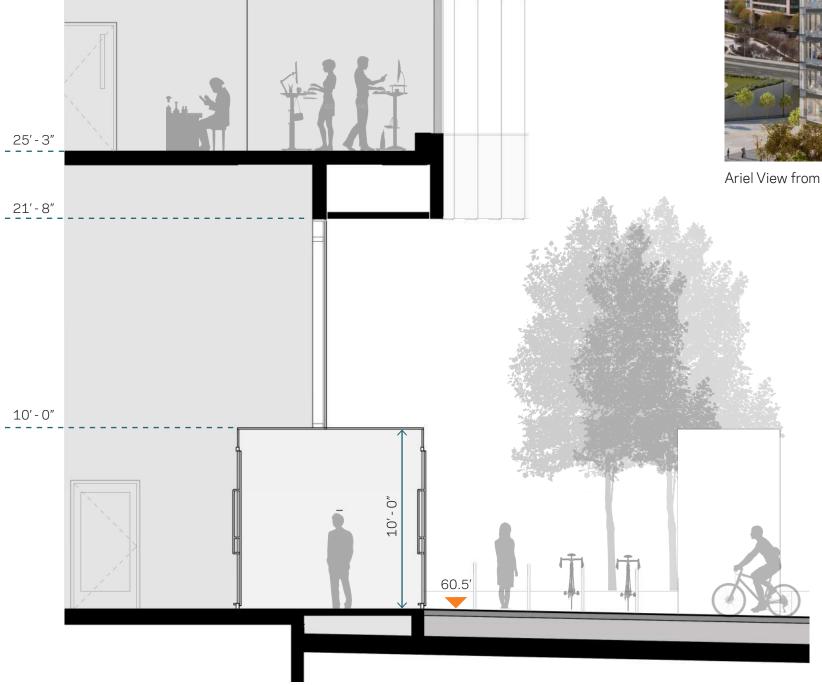
TRANSPARENCY
Facade system is highly
transparent to reveal the
lobby and denote the
entry

BUILDING INVITATION
Recessed building
massing revels a large
invitation soffit which pulls
eye site from the sidewalk
into the lobby

ENTRY FEATUREBuilding exhaust framed with signage or art instillation feature



DEXTER AVE. - FACADE EXPRESSION

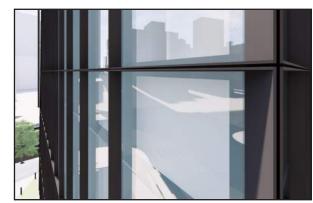




Ariel View from Dexter to SE corner

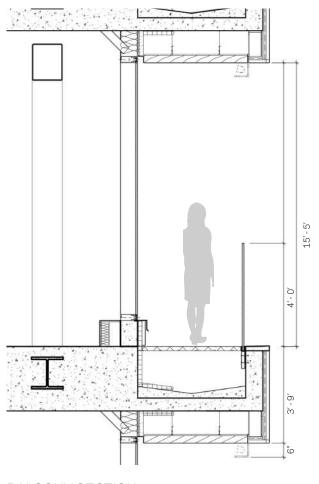


SE Corner Balconies





Enlarged Frame System View



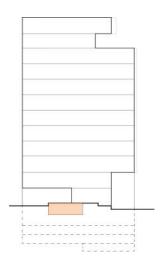


DEXTER AVE. - PROGRAMMING



PROGRAM KEY

- **1** Entry Vestibule
- **2** Lobby
- **3** FCC Room
- Reception
- Mail Room
- 6 Elevator Vestibule
- **7** Expressed Stair

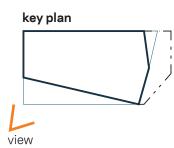


LOWER GROUND FLOOR PLAN

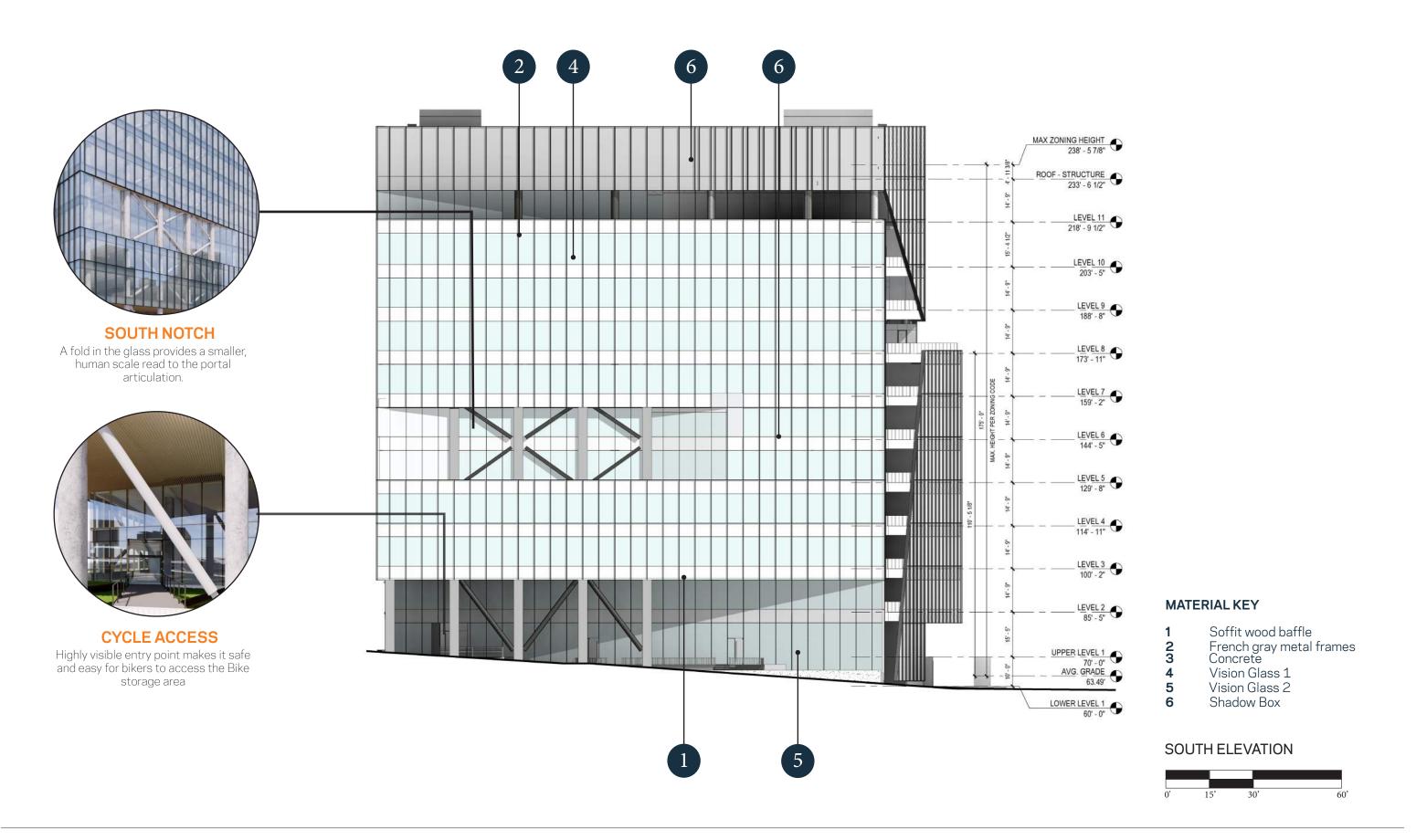






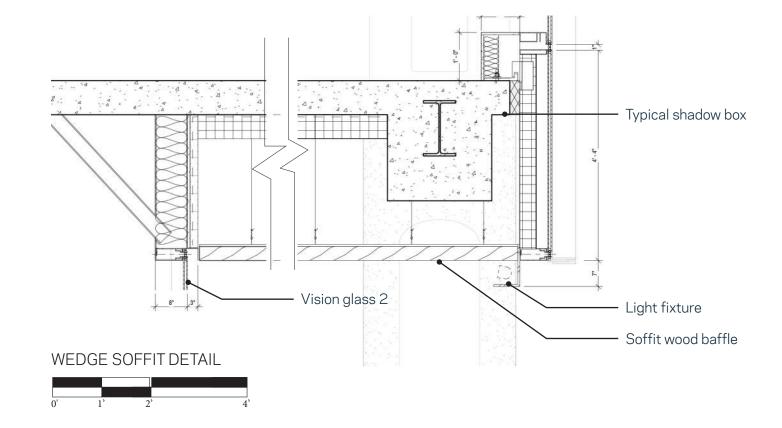


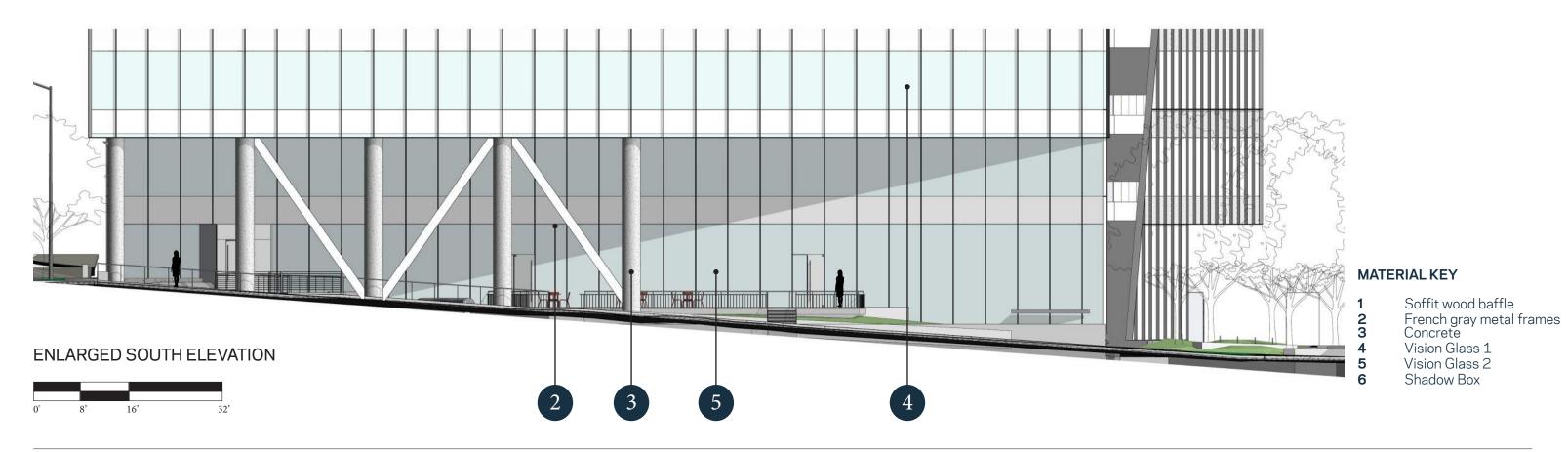
MERCER ST. - ELEVATION



MERCER AVE. - EXPERIENCE







MERCER AVE. - EXPERIENCE

The pedestrian experience from Aurora to Dexter is continually changing through diverse landscape, non-static water conditions, and a varying canopy depth.

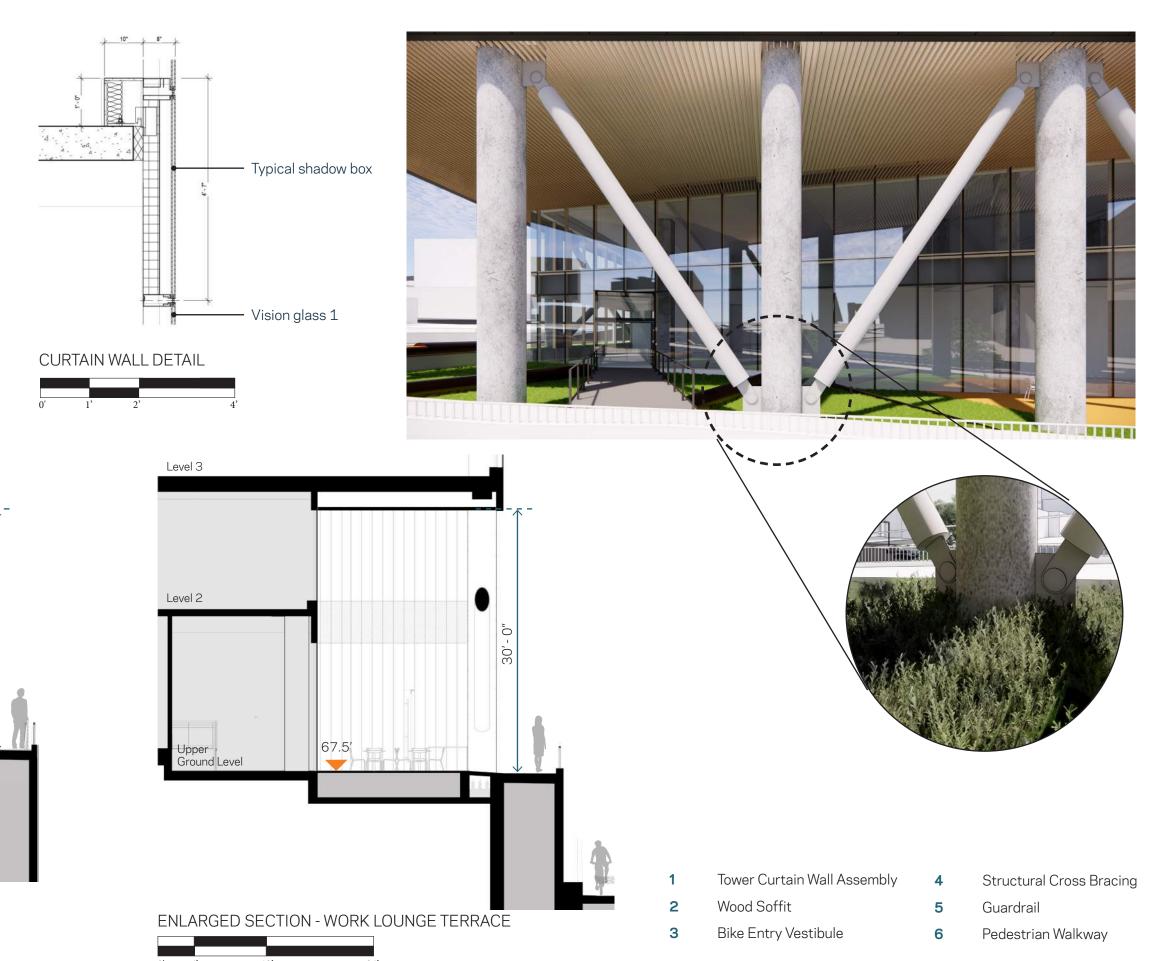
This dynamic zone is reinforced through spaces that support an active workplace. Beneath the canopy and adjacent to the landscape is a work lounge, occupiable terrace, and a bike entry accessible by bridge.

Level 3

Level 2

Upper Ground Level 70.0'

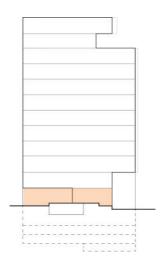
ENLARGED SECTION - BIKE BRIDGE

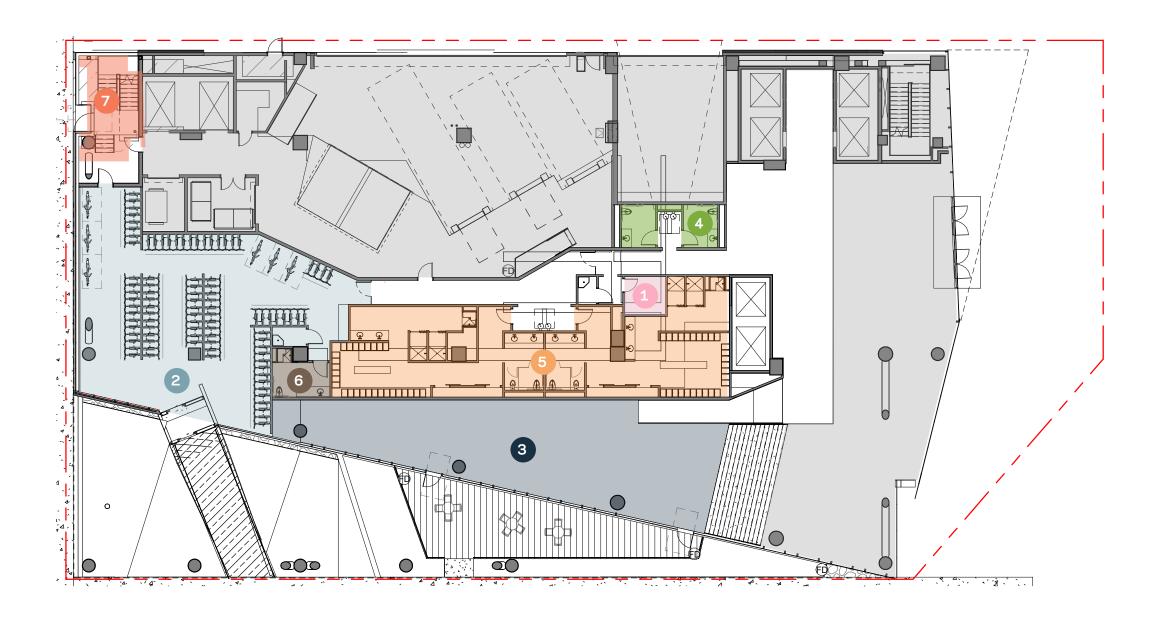


MERCER ST. - PROGRAMMING

PROGRAM KEY

- **1** Elevator Lobby
- **2** Bike Lobby
- **3** Restroom
- 4 Mother's Room
- **5** Women's Locker Room
- 6 Men's Locker Room
- **7** Work Lounge
- 8 Restroom / Shower
- **9** Bike Storage



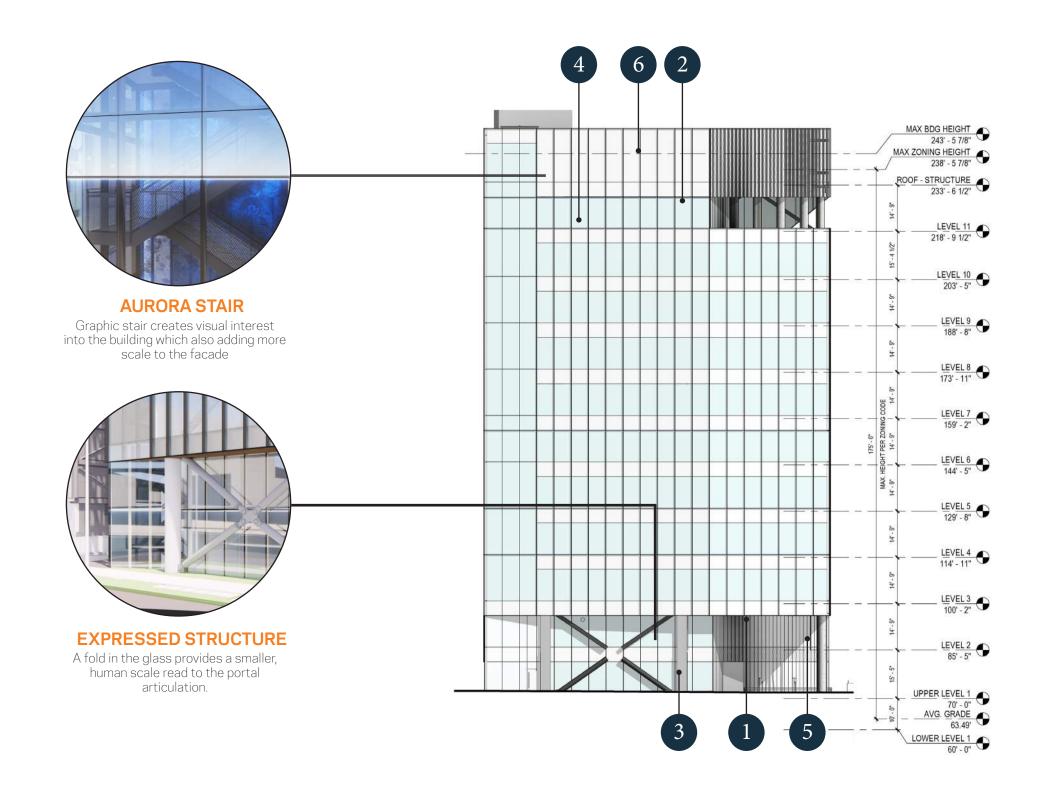


LOWER GROUND FLOOR PLAN





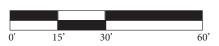
AURORA AVE. - ELEVATION



MATERIAL KEY

- Soffit wood baffle
- French gray metal frames Concrete 2
- Vision Glass 1
- Vision Glass 2
- Shadow Box

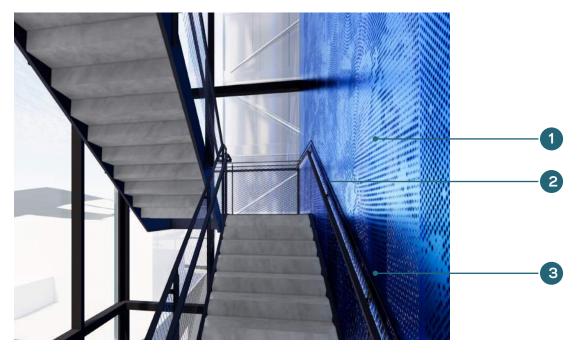
WEST ELEVATION



AURORA AVE. - STAIR EXPERIENCE



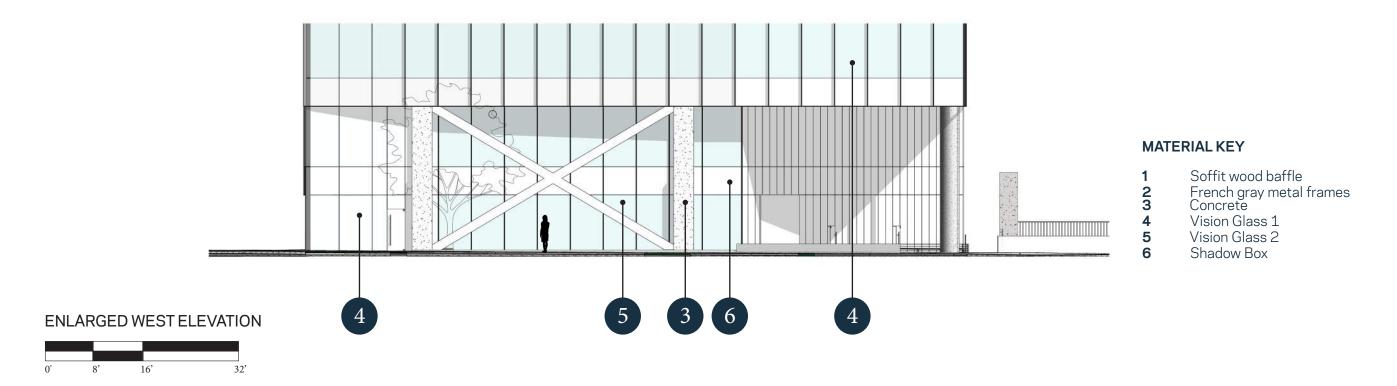




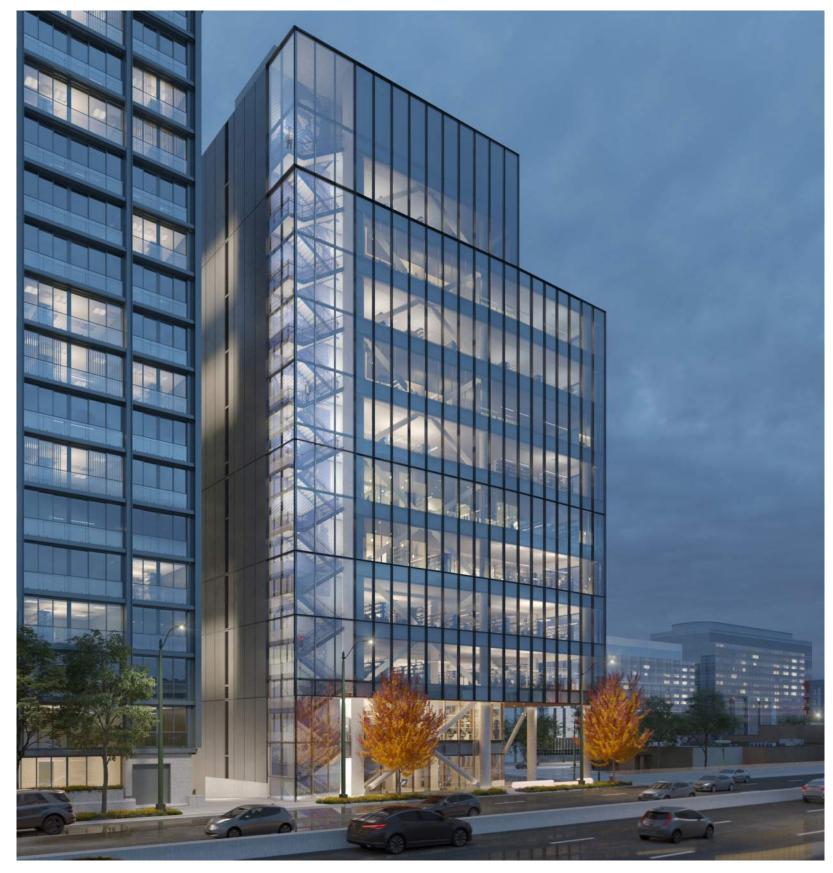
AURORA AVE.. - EXPERIENCE

ENGAGEMENT Graphic stair creates visual interest and encourages activation INVITATION Building massing pulls back creating invitation along Aurora and give a softer edge to the busy street ACCESSIBILITY Site Engagement for pedestrians at Bio retention

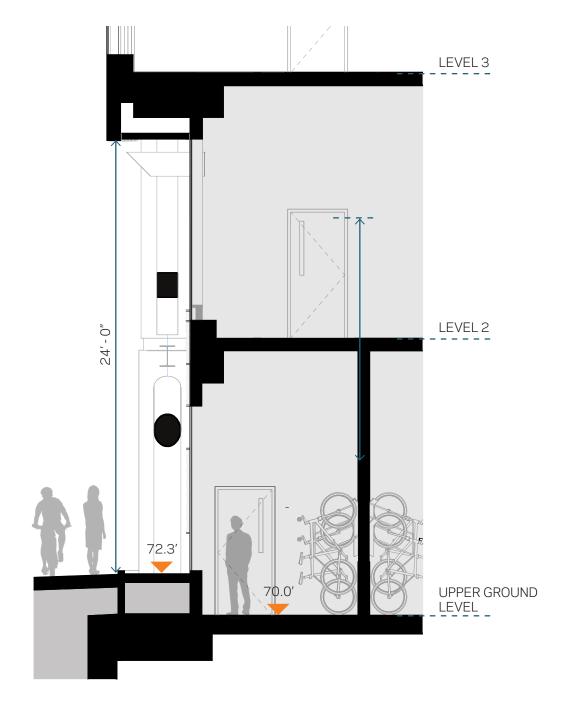
Aurora Street View to SW Corner



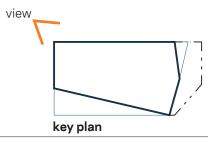
AURORA AVE. - FACADE EXPRESSION



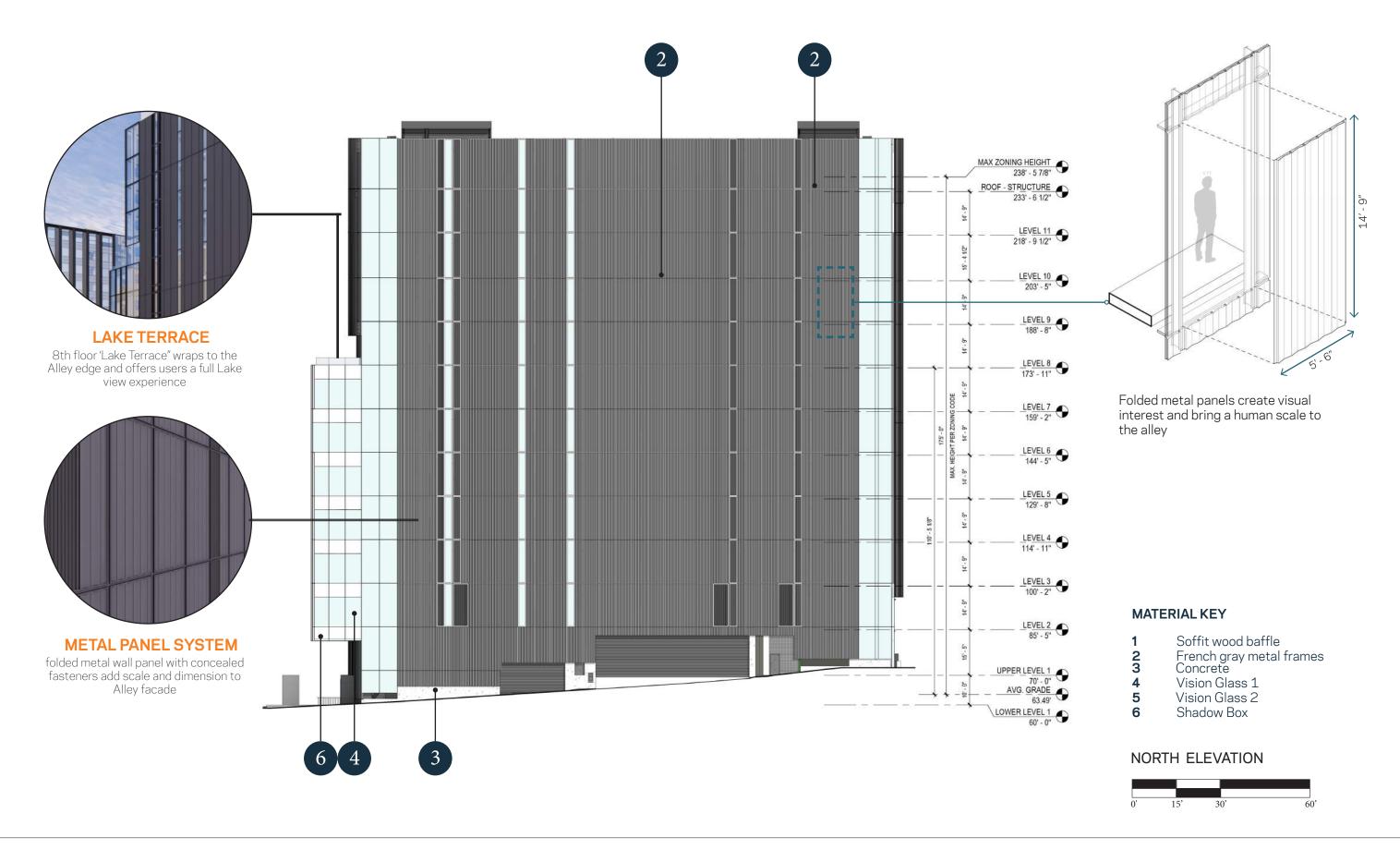
Aurora Street View to NW Corner





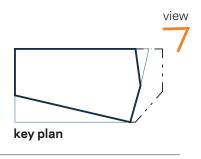


ALLEY - ELEVATION



ALLEY- STREET VIEW EXPERIENCE





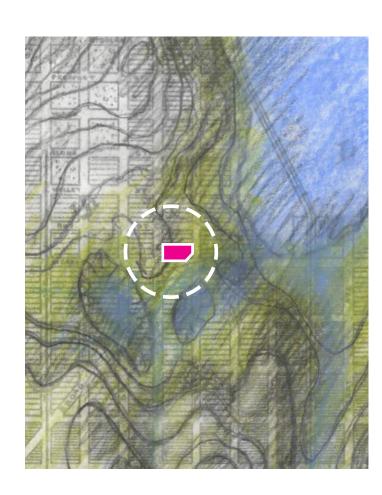
Dexter Street View to NE Corner

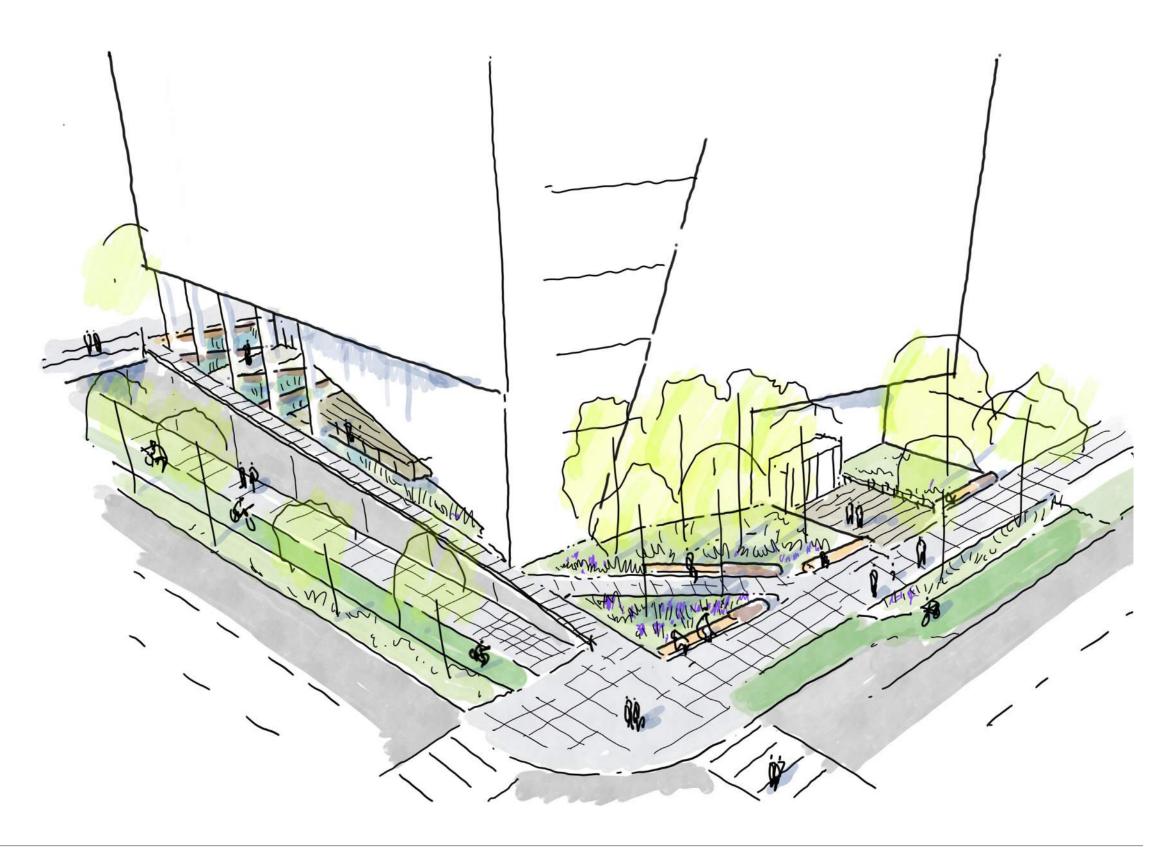
LANDSCAPE 06

LANDSCAPE GATEWAY: CONNECTING PEOPLE, TOPOGRAPHY, AND NATIVE ECOLOGY

The gateway intersection of Dexter Avenue and Mercer Street sits at a point that once marked a rise within the boggy extension of Lake Union. The site is now a constructed highpoint where the land falls toward the Westlake and Lake Union to the east and the cut of the Mercer Street underpass to the west.

The site design and building massing support this gateway by connecting the multiple surrounding scales, identities, modes of transportation, and views. At the corner of Mercer Street and Dexter Avenue the building massing works with the former Broad Street right of way to establish a larger landscape that relates to the larger legibility of this "high-ground gateway" and offer interest and amenity to pedestrians and cyclists at this busy intersection.





SITE PLAN



LANDSCAPE

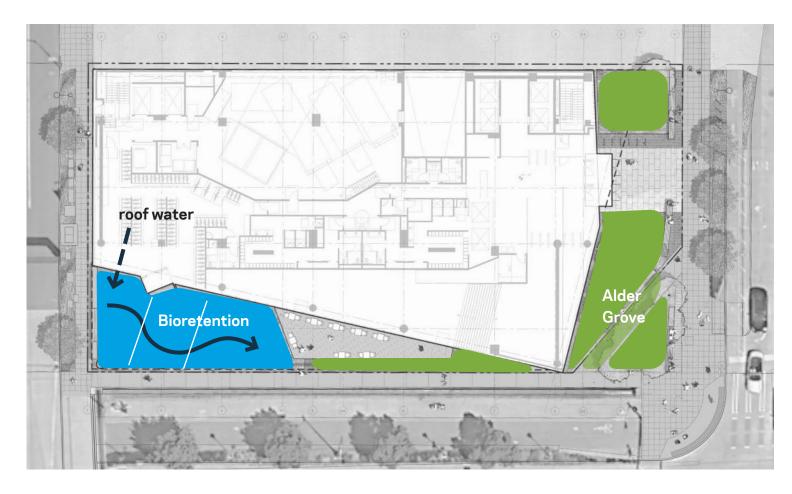
REGENERATIVE ECOLOGY ON DISPLAY











The building setback at the gateway corner provides an opportunity to create a strong planting anchor that can enhance the sustainability of the site and become a moment of relief for pedestrians at the busy intersection. Planting will feature native species that support the "high-ground gateway" identity and connect to the broader ecological context of what historically was a boggy ground near the shore of Lake Union and extending to today's Seattle Center. Focusing on species that support pollinators and birds will support South Lake Union's role as a connector between larger patches within the city.

Planting design will connect this unique constructed "high ground" with the historical "boggy" character. Native species will offer seasonal interest, pollinator benefits, and build soil health on this disrupted site.

CONNECTING THROUGH THE LAND





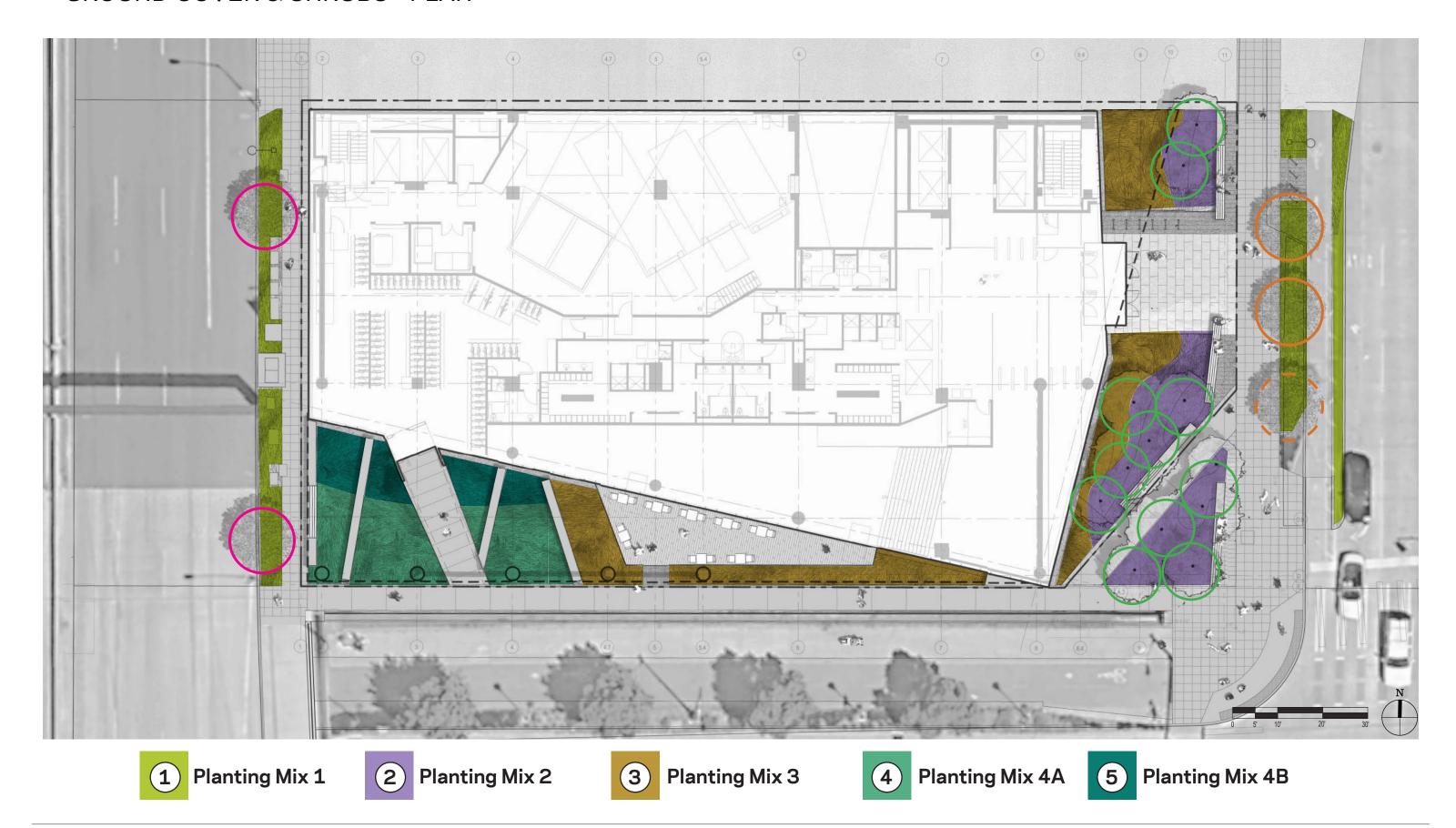








GROUND COVER & SHRUBS - PLAN



PLANTING - SPECIES







Quercus Bicolor 'American Dream' Swamp White Oak



Alnus Rubra Red Alder

Planting Mix 1



Camassia leichtlinii Symphoricapos



Fragaria chiloensis Achillea millefolium





lanatum



2 Planting Mix 2













lemmonii

Planting Mix 3





albus









Carex densa

Planting Mix 4A





pachystachya



cespitos









Symphoricapos

Planting Mix 4B





pachystachya













SITE MATERIALS



SITE MATERIALS







SDOT standard concrete pavement



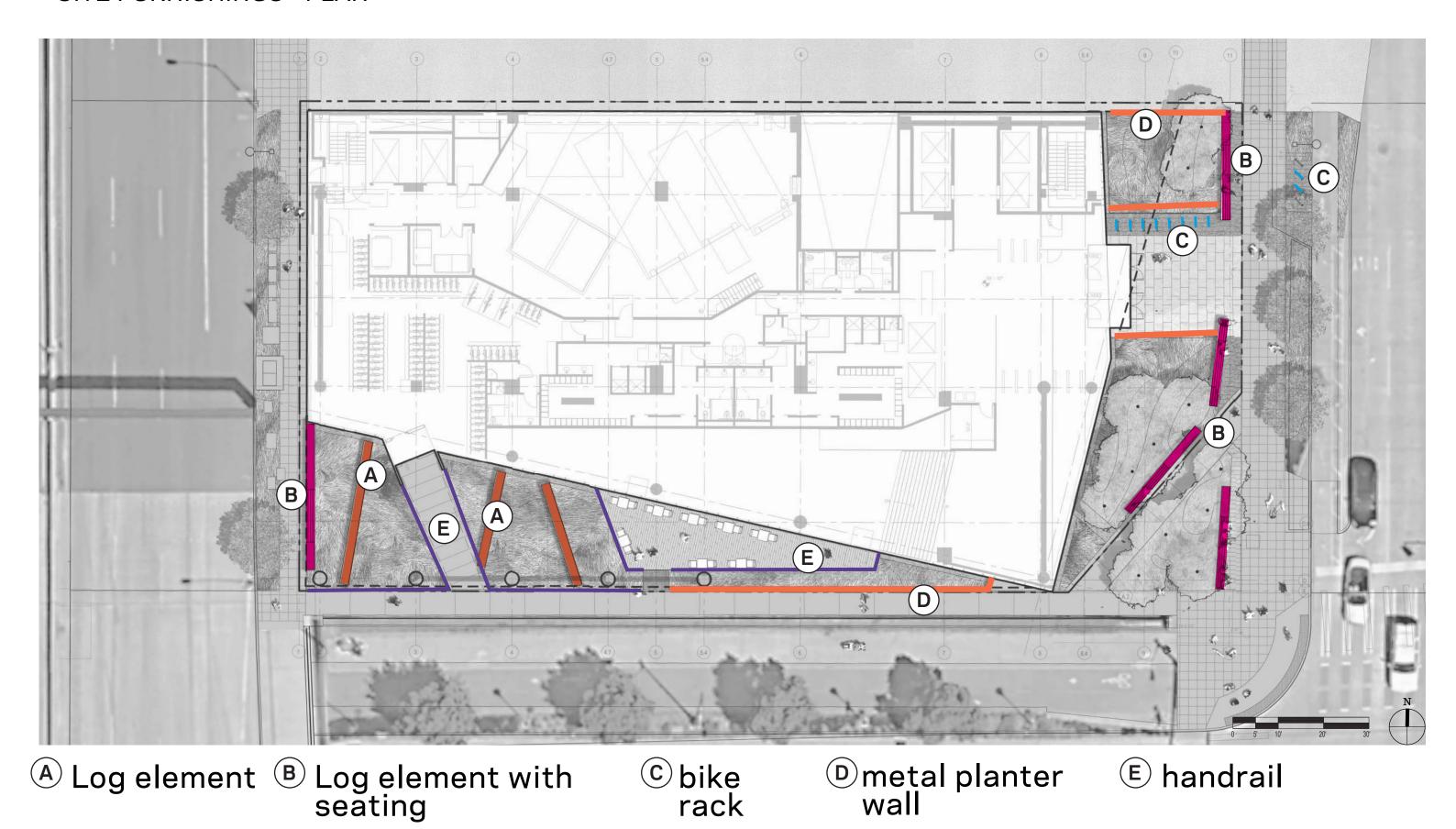






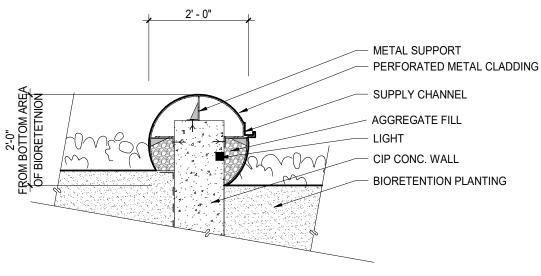
SDOT standard asphalt pavement

SITE FURNISHINGS - PLAN

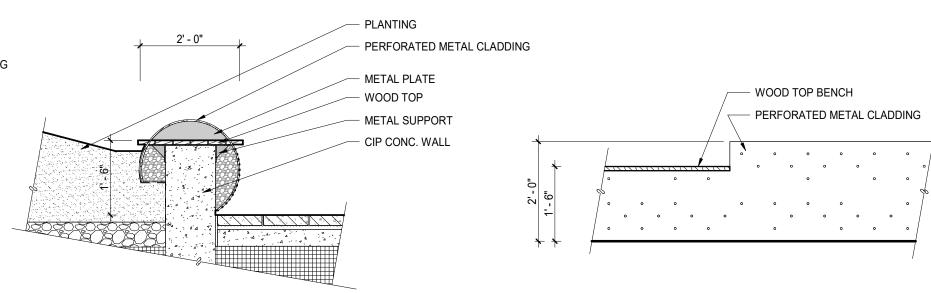


SITE FURNISHINGS - DETAILS

A Log element (custom)



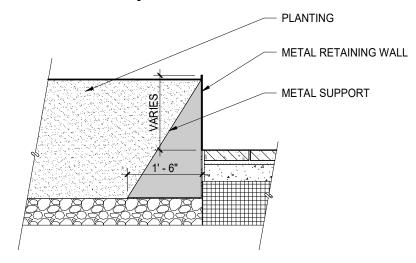
B Log seating wall (custom)



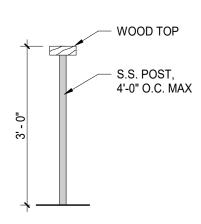
© bike rack



(D) metal planter wall



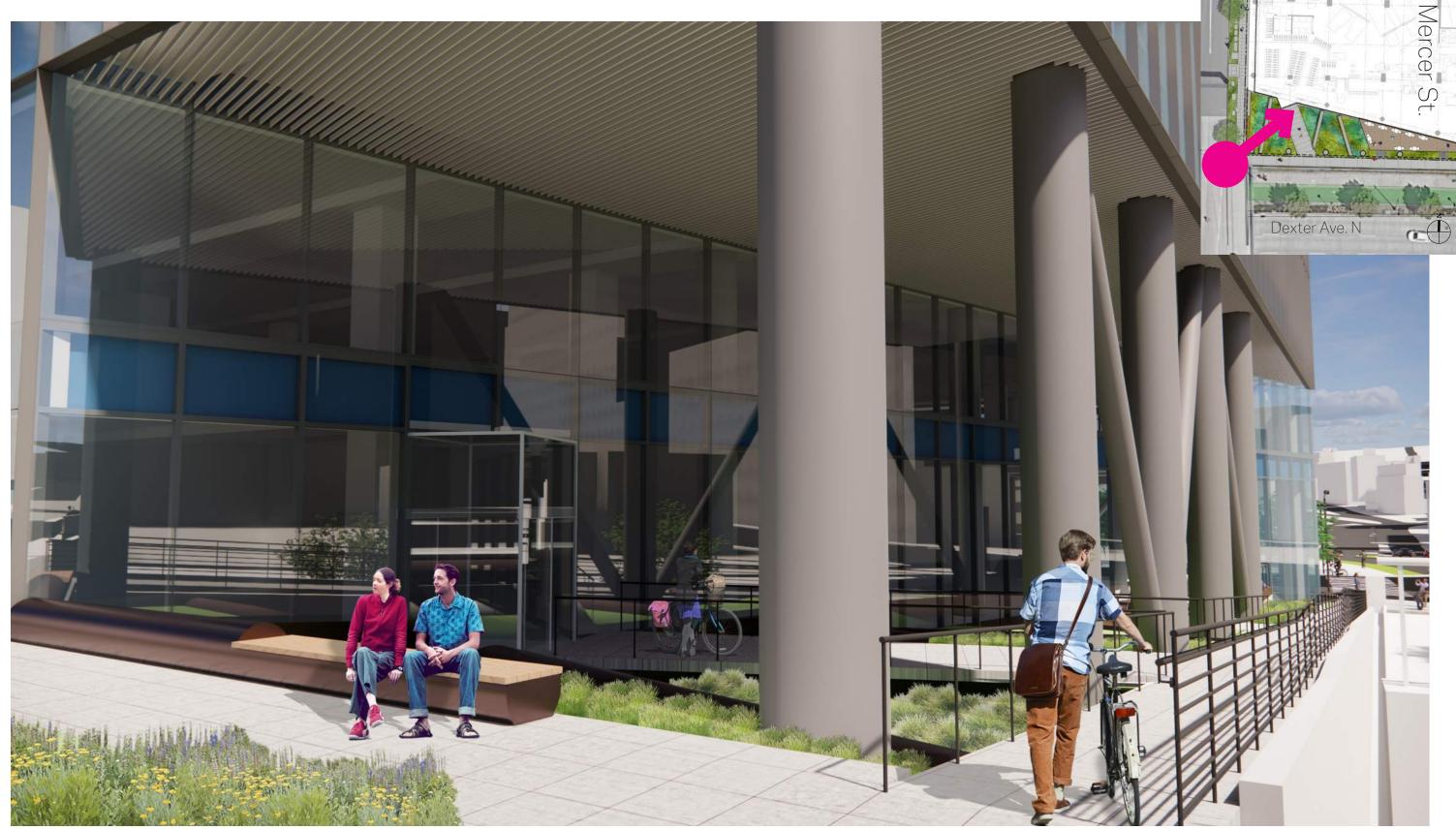
E handrail



DEXTER VIEW



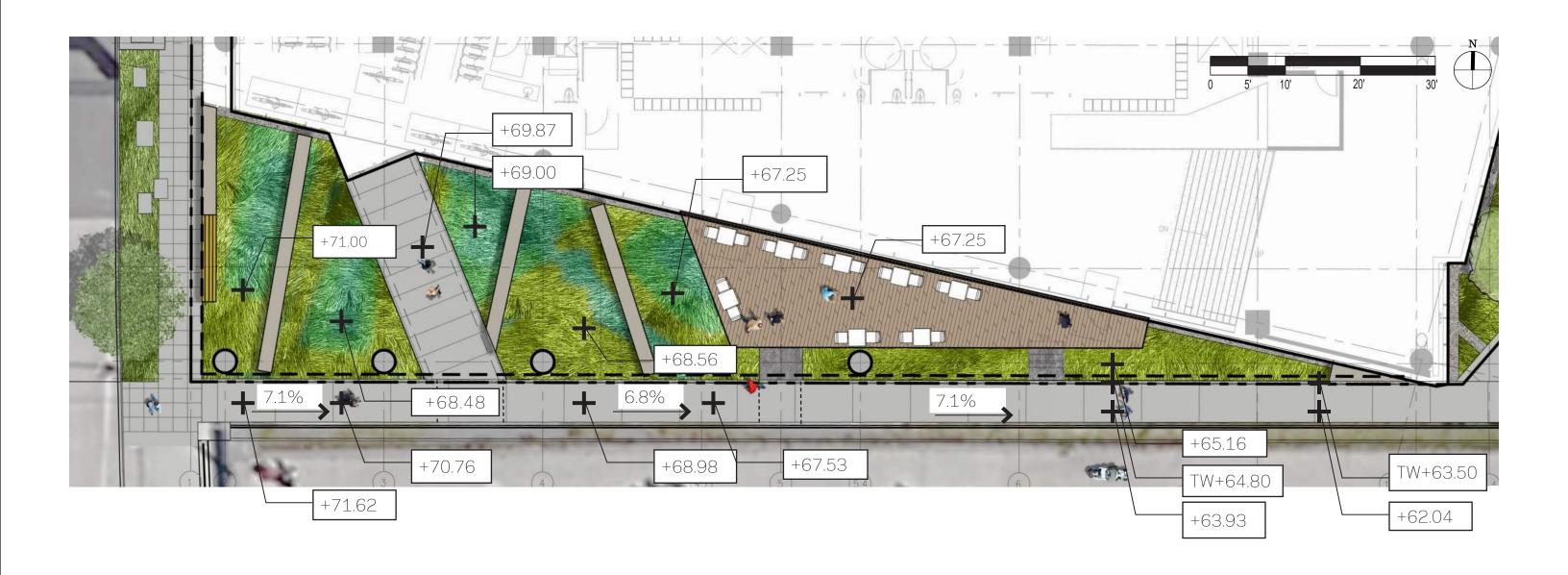
AURORA AVE N.



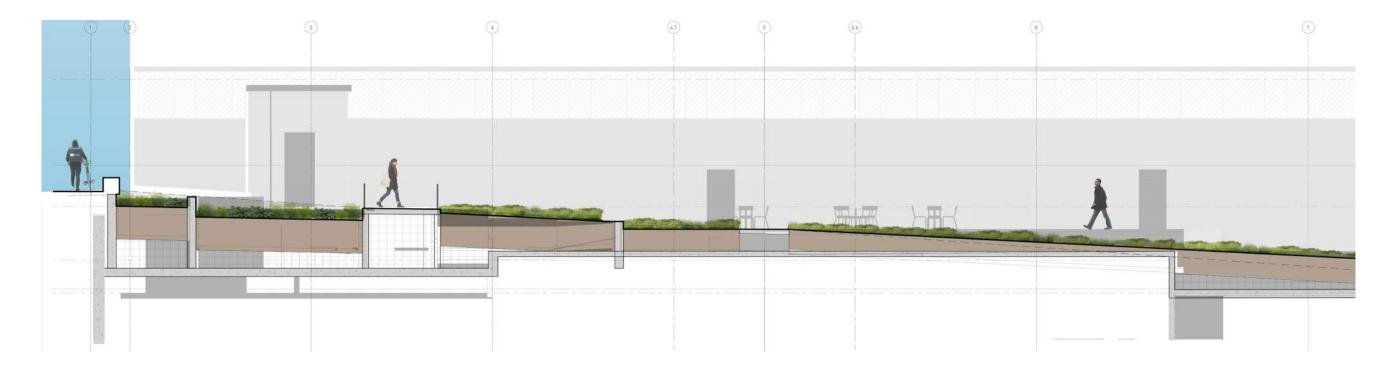
SECTIONS

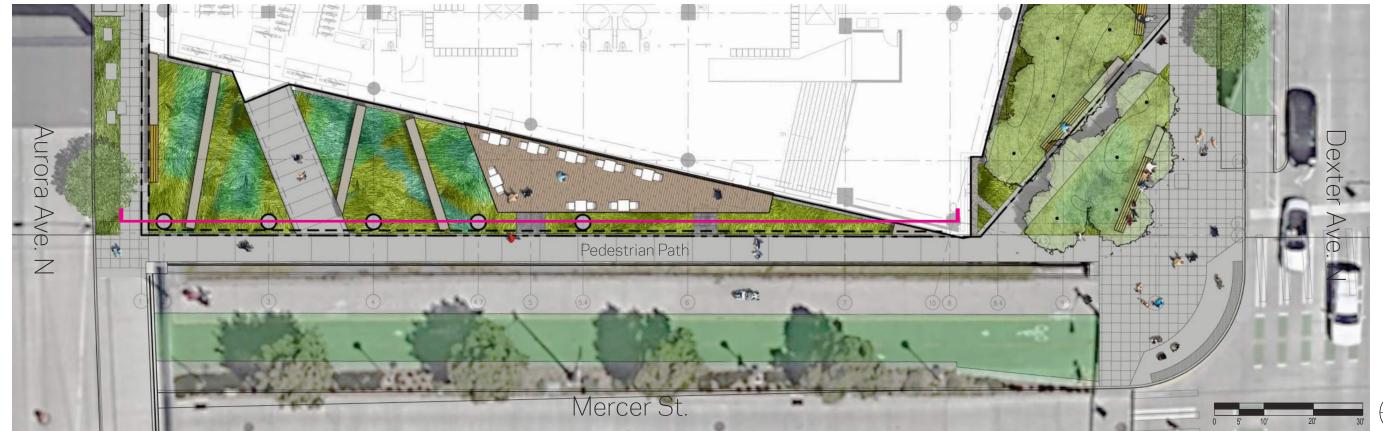


AURORA AVE N.



MERCER AVE







LIGHTING / SIGNAGE

SITE LIGHTING'

Lighting for the site will support and enhance both the building identity and wayfinding. The feature art under the overhang will be highlighted to draw visitors from afar, with moments of discovery at the personal level as cyclists and pedestrians pass by. The main entry will be will illuminated with a feature pole to enhance the perception of safety and enhance light levels at the front door. Lighting integrated to the benches illuminate the secondary cut-through path without visually competing. No tree uplights are used.



PATHWAY MOMENTS

HIGHLIGHTING LOGS AND BIKE PATHWAYS



PEDESTRIAN POLE + BOLLARDS

ENHANCES PERCEPTION OF SAFETY

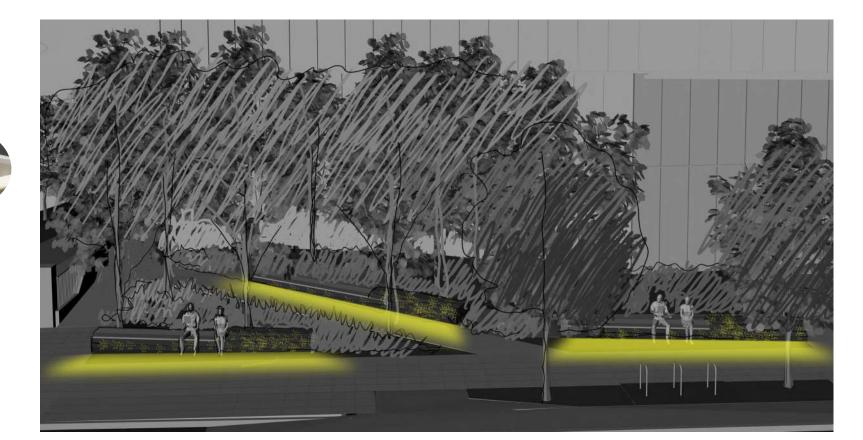


CONCEALED LIGHTING

ILLUMINATE THE PATH WITHOUT DRAWING EXTRA ATTENTION

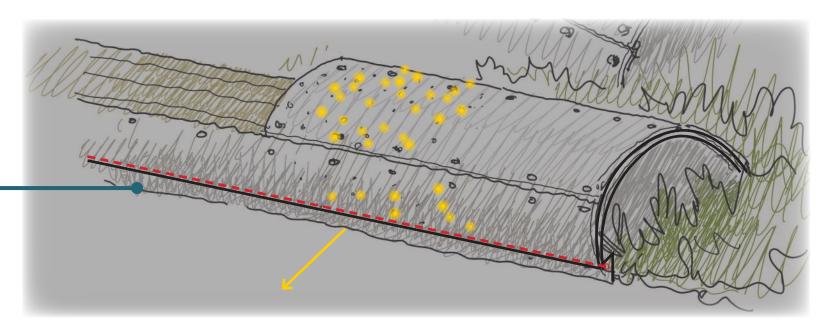


Benches
lighting integrated
to bottom
to illuminate
secondary paths





Concealed
A niche carved away (6" Tall) to hide a grazer



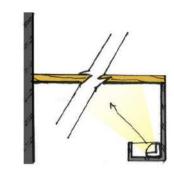
SOFFIT UPLIGHTING

SOFT UPLIGHTING AT SOFFIT TO HIGHLIGHT ARCHITECTURE

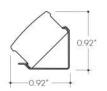


*note: renderings are provided to convey design concepts; final illuminance may differ from renderings













COLUMN LIGHTING

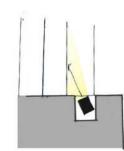
HIGHLIGHT COLUMNS AND CROSS BRACING



*note: renderings are provided to convey design concepts; final illuminance may differ from renderings

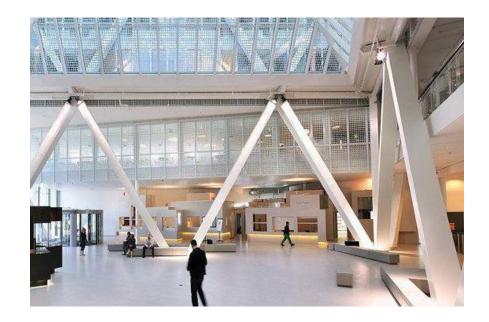












IDENTITY & SIGNAGE

SIGNAGE OVERVIEW





- 1 Upper Building Tenant Identity
- 2 Building Entrance Identity
- 3 Building Address

IDENTITY & SIGNAGE

SIGNAGE PRECEDENTS

1 Upper Building Tenant Identity















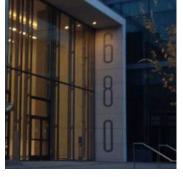




3 Building Address









IDENTITY & SIGNAGE IDENTITY & SIGNAGE

SIGN REGULATIONS

The on-premises building identity signage is appropriately scaled and detailed to fit within Seattle sign regulations, as well as fit the context of the building facades.

The overall signage area for each building identity will be no more than 672 sf per sign, per elevation. This approach is based on the building having multiple major tenants.







- 1 Upper Building Tenant Identity
- 2 Building Entrance Identity
- 3 Building Address

IDENTITY & SIGNAGE

SIGN REGULATIONS

At the primary entry, the vestibule works as an ideal location for building signage. Near the sidewalk, a generator exhaust shaft will be treated with art and serve as a marker for those arriving to the property.

Signage will also denote the entry for cyclists.



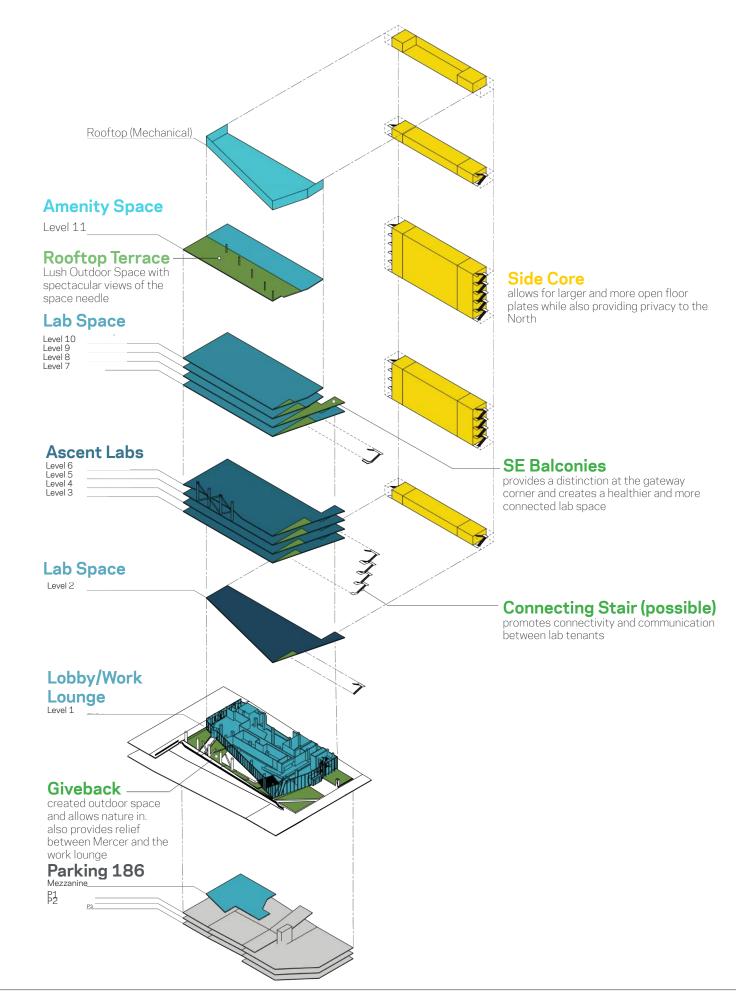


- 1 Upper Building Tenant Identity
- 2 Building Entrance Identity
- 3 Building Address

PLANS/SECTIONS 08

PROJECT PARTS

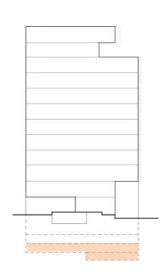
USES AND STACKING

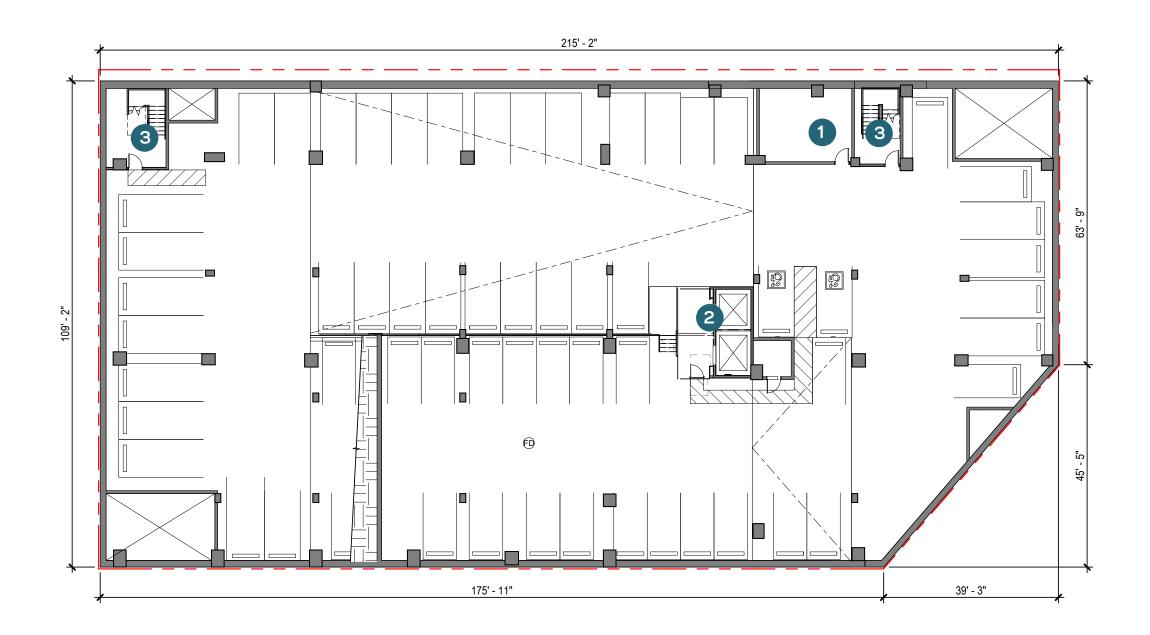


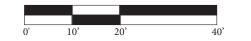
FLOOR PLAN LEVEL P3

- 1 Energy Storage
- **2** Elevator Vestibule
- **3** Stair Core

Open to below



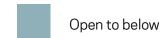


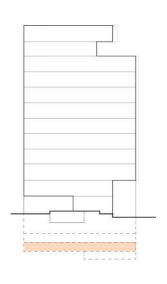


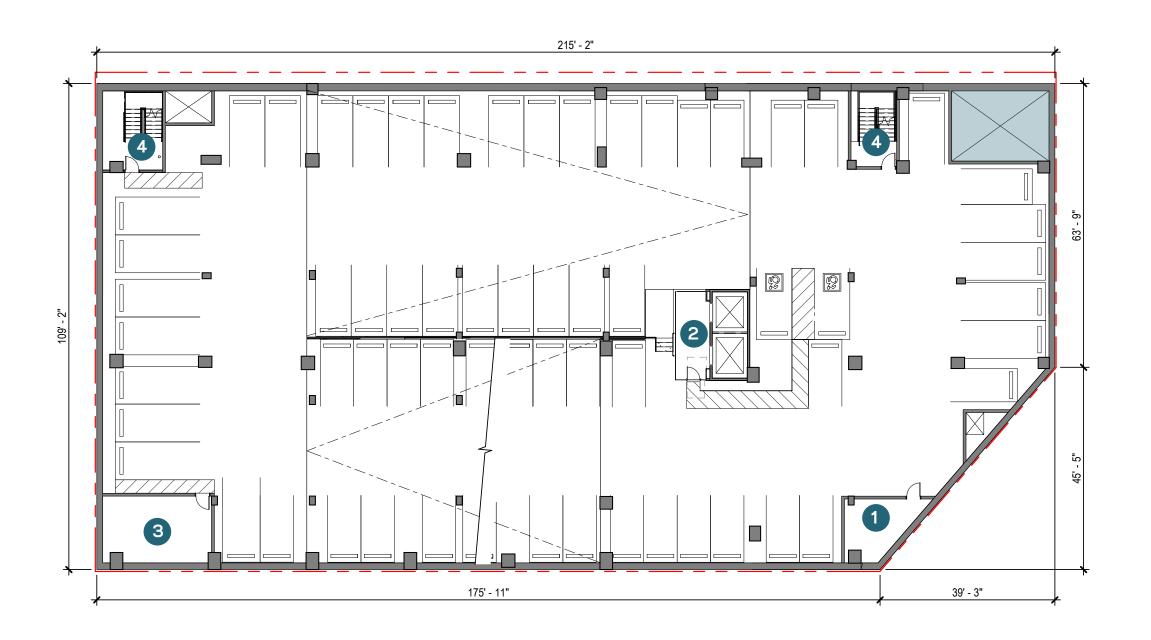


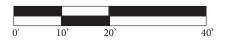
FLOOR PLAN LEVEL P2

- 1 Electrical
- **2** Elevator Vestibule
- **3** Water Entry
- **4** Stair Cores



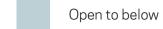


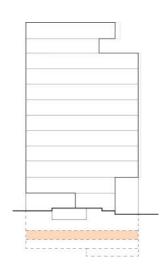


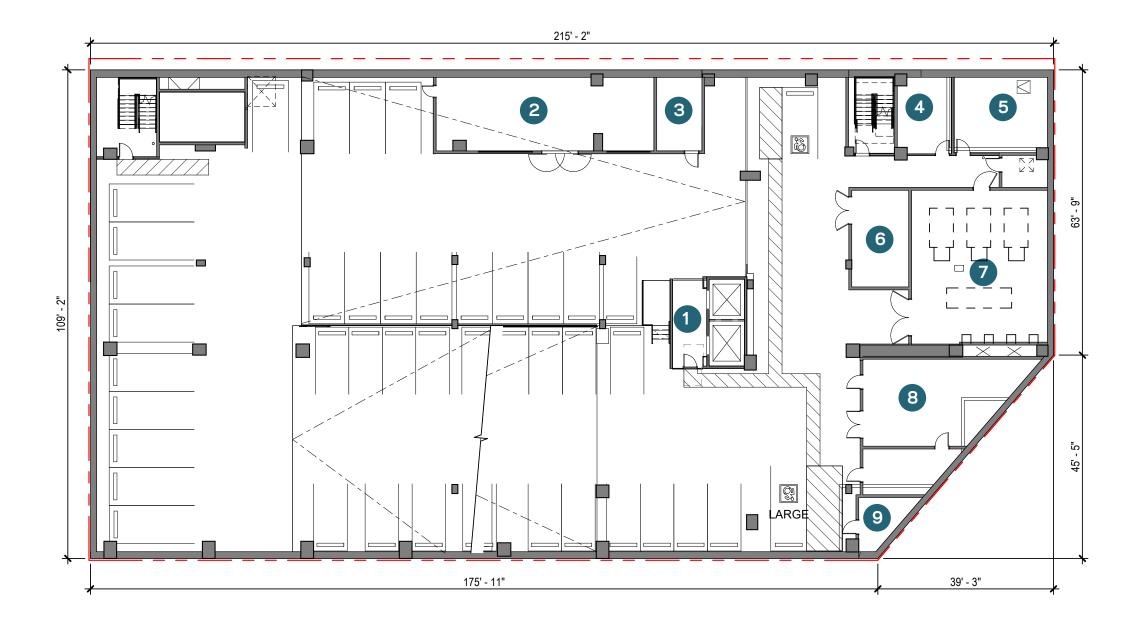


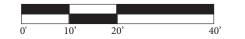
FLOOR PLAN LEVEL P1

- **1** Elevator Vestibule
- **2** Emergency Generator
- **3** MPOE
- 4 Heat Exchange Room
- **5** Fire Pump Room
- 6 Switch Room
- **7** Transformer Vault
- 8 Main Electrical Room
- **9** MPOE
- 10 Stair Cores







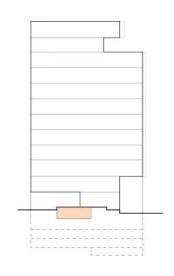


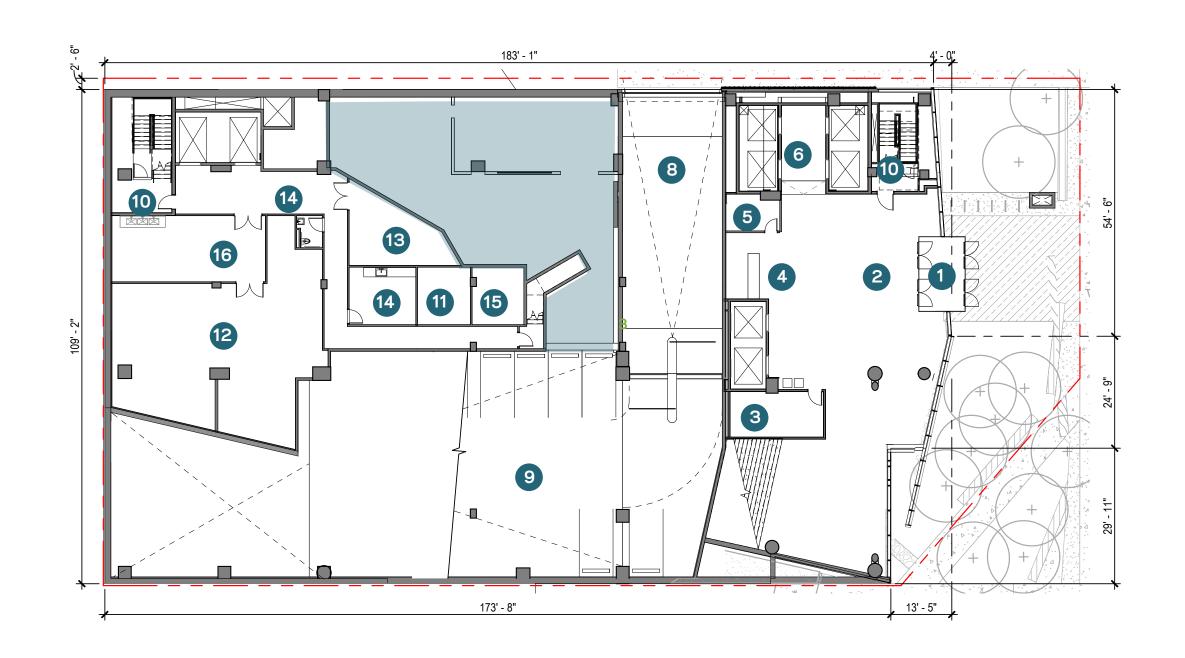


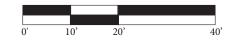
FLOOR PLAN LEVEL L1

- Entry Vestibule
- 2 Lobby
- FCC Room
- 4 Reception
- Mail Room
- Elevator Vestibule
- Work Lounge
- 8 Parking Ramp
- Parking
- Stair Cores
- Dock Master
- Glasswash / Autoclave
- Maintenance Room
- Break Room
- MDF Room





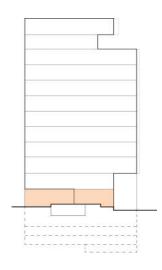


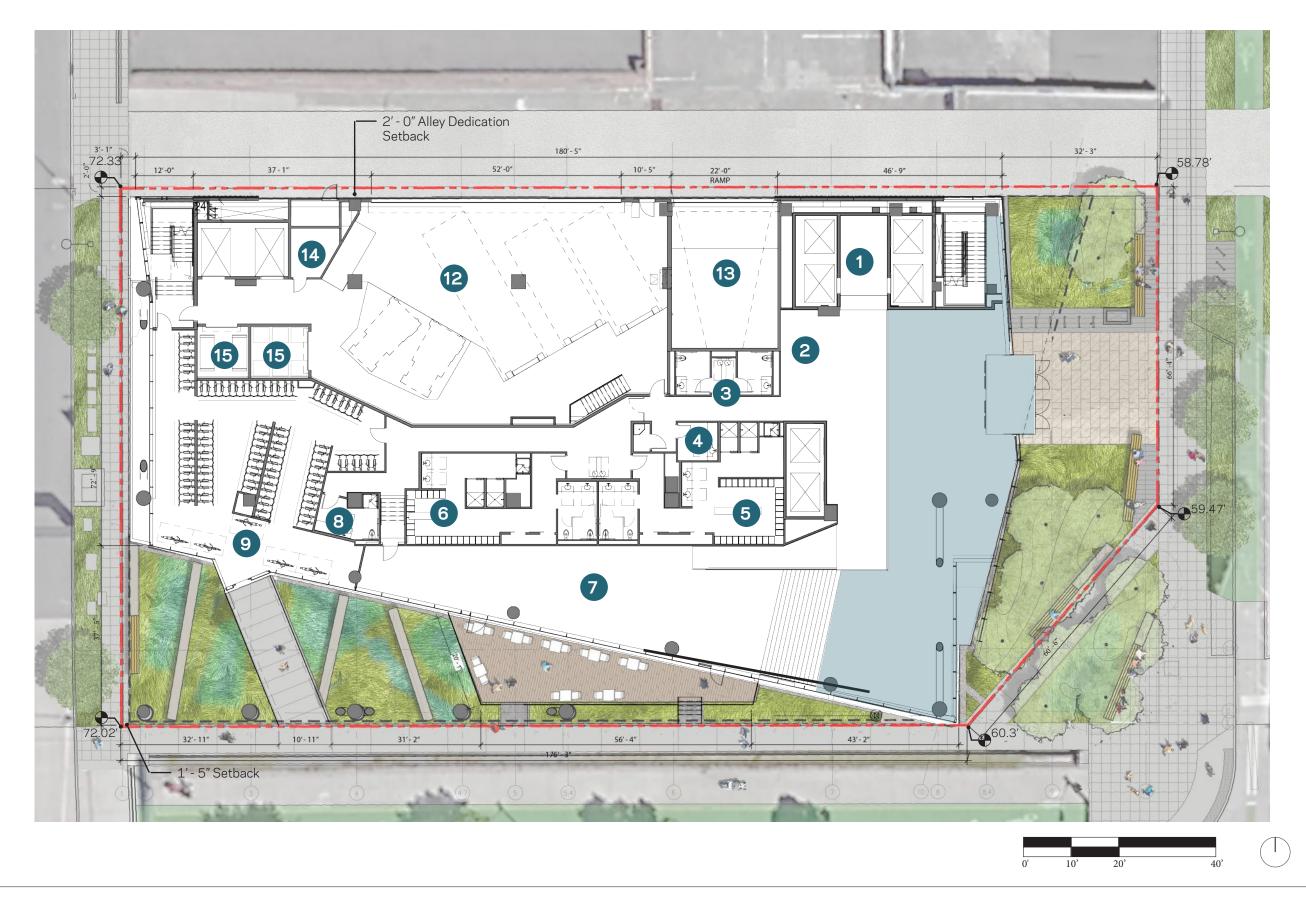


FLOOR PLAN LEVEL U1

- **1** Elevator Lobby
- **2** Bike Lobby
- 3 Restroom
- 4 Mother's Room
- 5 Women's Locker Room
- 6 Men's Locker Room
- **7** Work Lounge
- 8 Restroom / Shower
- **9** Bike Storage
- **10** Stair Cores
- **12** Loading Dock
- **13** Parking Ramp
- **14** Package Room
- **15** Trash Room



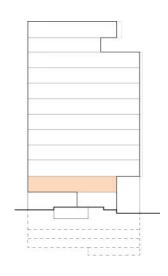


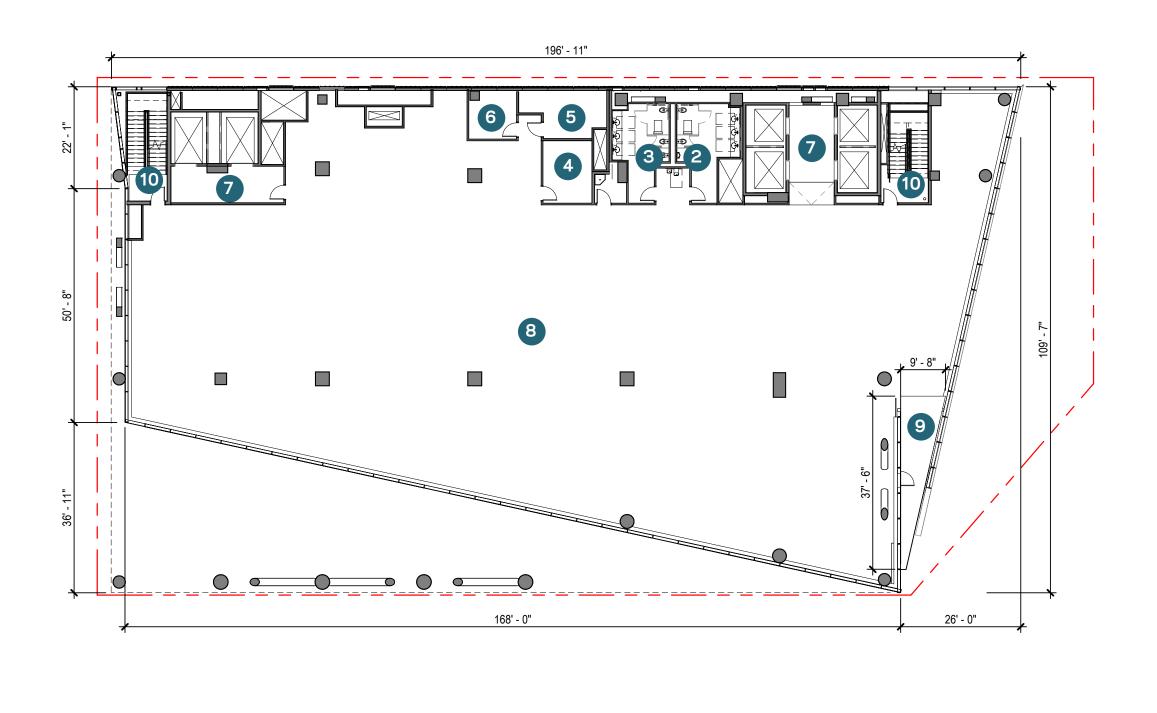


FLOOR PLAN LEVEL 02

- **1** Elevator Lobby
- 2 Men's Restroom
- **3** Women's Restroom
- **4** Storage
- **5** Electrical Room
- 6 IDF Room
- **7** Elevator Vestibule
- 8 Lab / Office
- 9 Balcony
- **10** Stair Cores

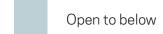


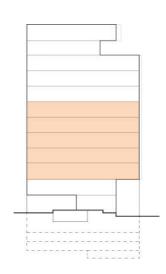




FLOOR PLAN LEVEL 03-07

- Elevator Lobby
- 2 Men's Restroom
- Women's Restroom
- Storage
- Electrical Room
- 6 IDF Room
- Elevator Vestibule
- 8 Lab / Office
- Balcony
- Stair Cores





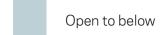


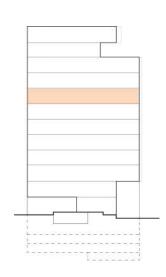




FLOOR PLAN LEVEL 08

- Elevator Lobby
- 2 Men's Restroom
- Women's Restroom
- Storage
- Electrical Room
- 6 IDF Room
- Elevator Vestibule
- 8 Lab / Office
- Balcony
- Stair Cores



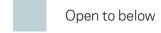


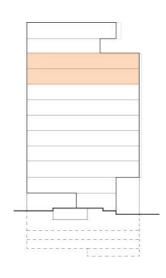


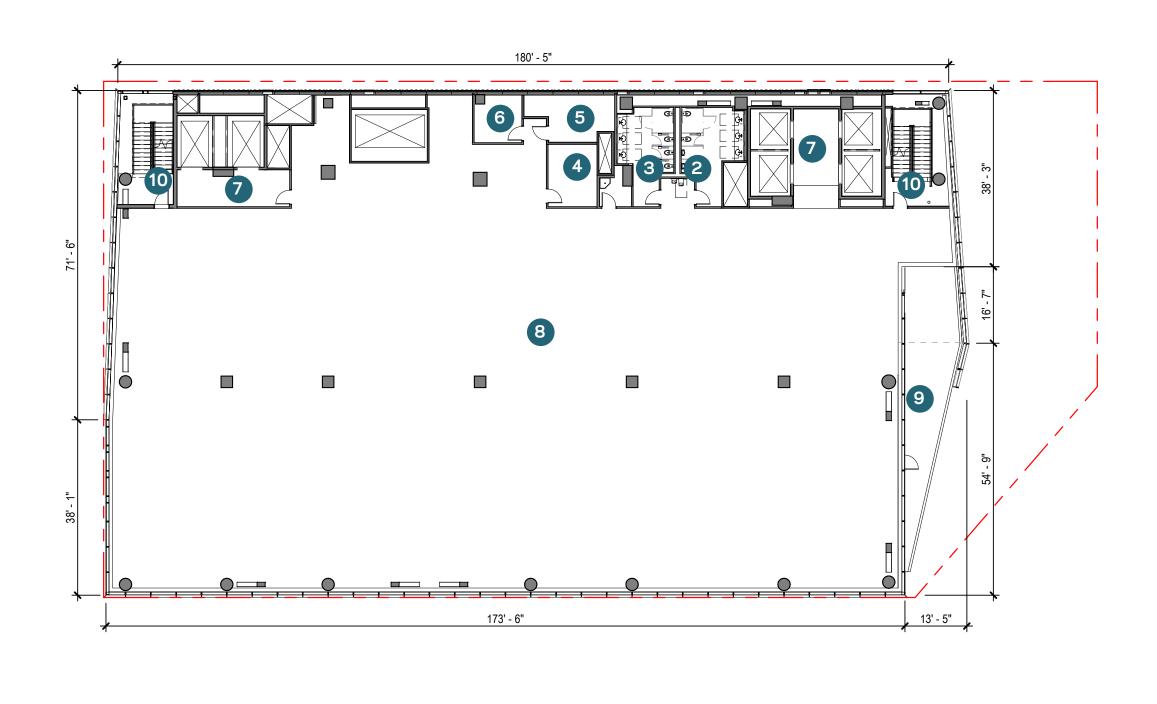


FLOOR PLAN LEVEL 09-10

- Elevator Lobby
- 2 Men's Restroom
- Women's Restroom
- Storage
- Electrical Room
- 6 IDF Room
- Elevator Vestibule
- 8 Lab / Office
- Balcony
- Stair Cores







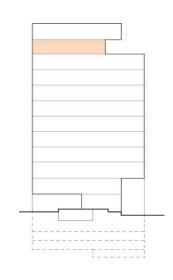


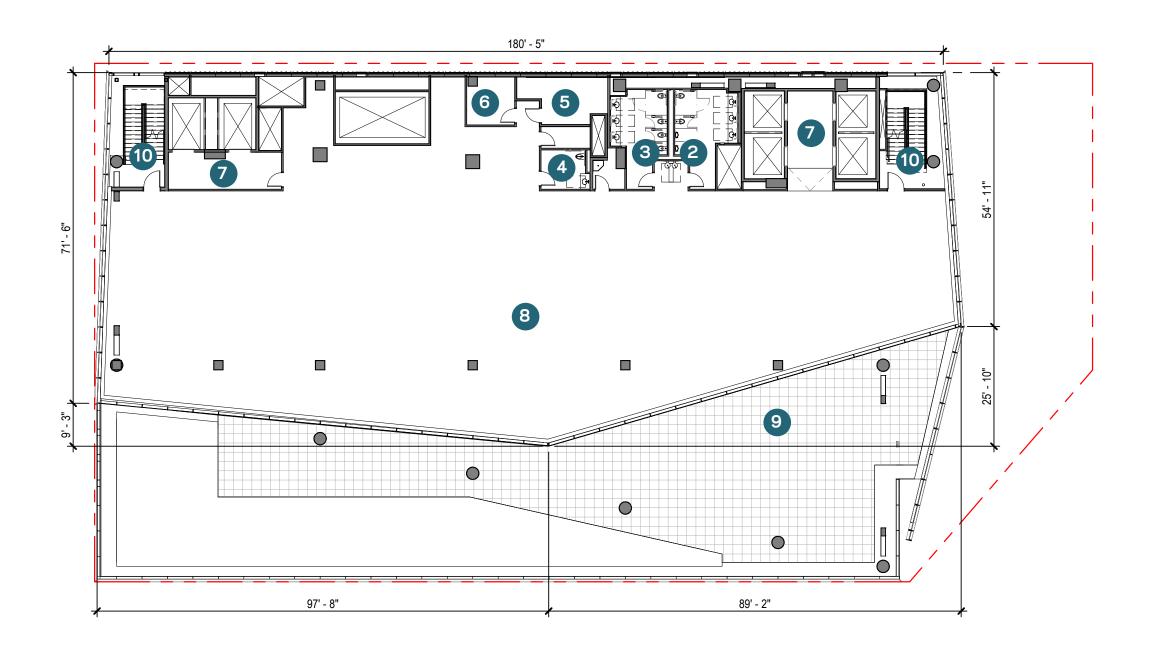


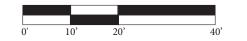
FLOOR PLAN LEVEL 11

- **1** Elevator Lobby
- 2 Men's Restroom
- **3** Women's Restroom
- 4 Uni-sex Restroom
- **5** Electrical Room
- 6 IDF Room
- **7** Elevator Vestibule
- 8 Lab / Office Amenity
- **9** Amenity Terrace
- **10** Stair Cores



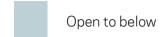


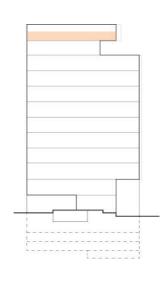


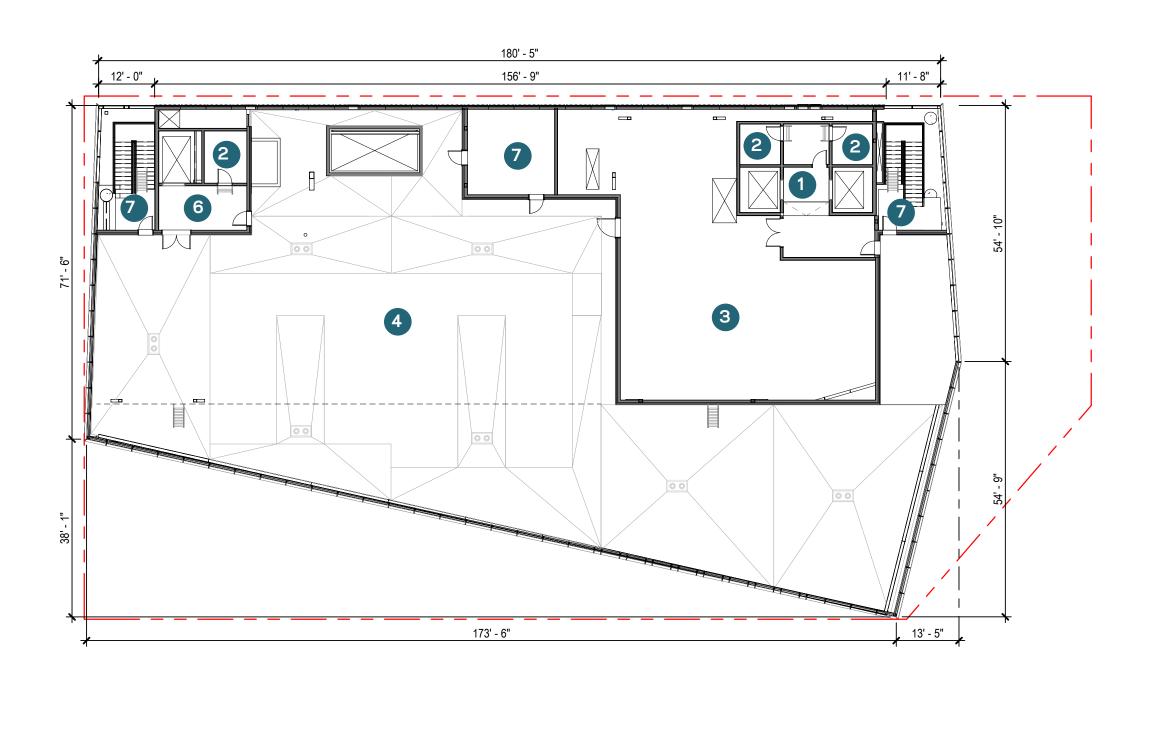


FLOOR PLAN LEVEL ROOF

- **1** Elevator Lobby
- 2 Elevator Control Room
- 3 Indoor Mechanical Room
- 4 Outdoor Mechanical Room
- **5** Electrical Room
- **6** Elevator Vestibule
- **7** Stair Cores







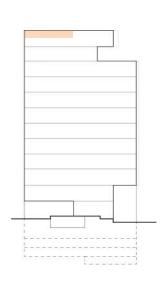


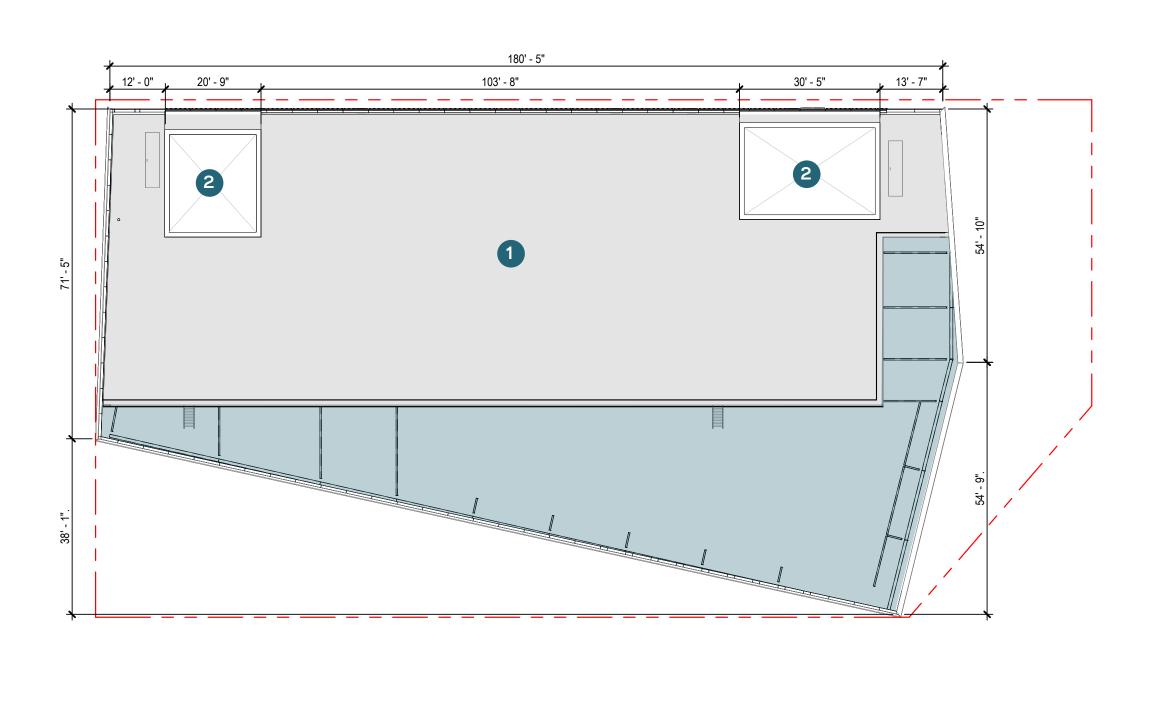


FLOOR PLAN LEVEL UPPER ROOF

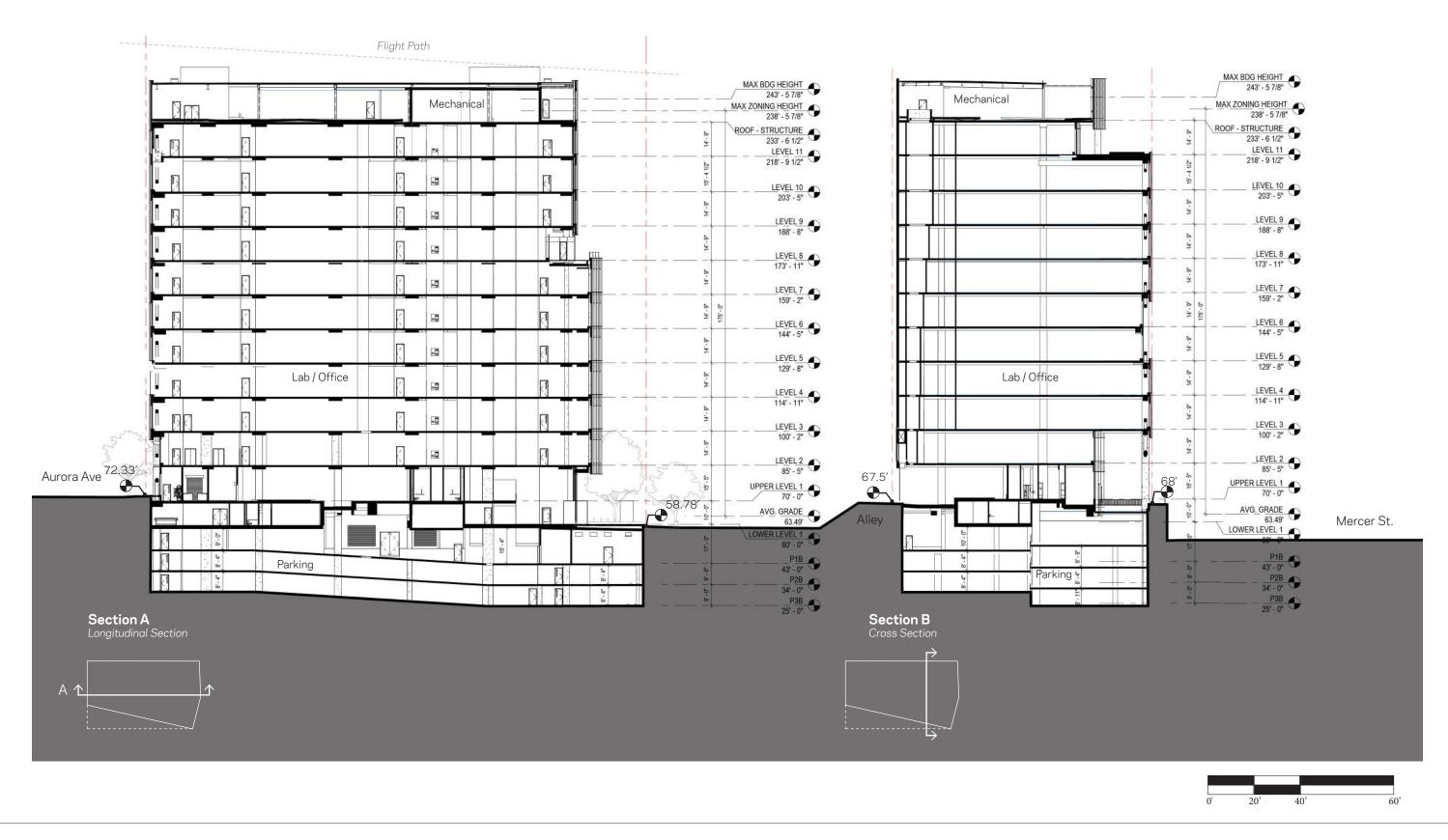
- 1 Outdoor Mechanical Platform
- 2 Elevator Overhead







SECTIONS



DEPARTURES 09

DEPARTURE SUMMARY

Requirement

Request

DEPARTURE #1 (SUPPORTED BY EDG)

23.48.025.c.5.b

Rooftop feature coverage, elevator penthouses and mechanical equipment height

23.48.025.c.7.b

Rooftop feature coverage and distance to roof edge

If elevator provides access to a rooftop designed to provide usable open space, elevator penthouses and mechanical equipment up to 45 feet above maximum height limit are permitted, provided the structure must be greater than 125 feet in height, and combined total roof coverage of feature gaining additional height does not exceed limits listed in 23.48.025.c.4.

At the applicant's option, combined total coverage of all features listed in subsections 23.48.025.c.4 and 23.48.025.c.5 may be increased to 65 percent of the roof area, provided that a. all mechanical equipment is screened; and b. no rooftop features are located closer than 10 feet to the roof edge.

Request to extend coverage percentage of total roof features from 65% to 100%, and relief from the 25% limit of total roof coverage of feature gaining additional height. request also to allow the rooftop features to be located closer than 10 feet to the roof edge.

DEPARTURE #2 (SUPPORTED BY EDG)

23.48.240.B.1b

Maximum 12' setback along Dexter and Mercer

The street-facing facade of a structure may be set back up to 12 feet from the street lot line subject to the following (Exhibit B for 23.48.240)

Request for setting back the facade of the proposed project more than 12 feet over 30% of lot line along Dexter Ave for an approximate average of 25 feet setback. The requested setback will be located within 20 feet of street corner.

DEPARTURE #3

23.54.030.B.2c

Parking Requirement, non-residential

When 20 or more parking spaces are provided, a minimum of 35 percent of the parking spaces shall be striped for small vehicles. The minimum required size for small parking spaces shall also be the maximum size. A maximum of 65 percent of the parking spaces may be striped for small vehicles. A minimum of 35 percent of the spaces shall be striped for large vehicles.

Request for reducing the percentage of large vehicles spaces from 35% to 20%

DFPARTURF #1

23.48.025.c.5.b - Rooftop feature coverage, elevator penthouses and mechanical equipment height

23.48.025.c.7.b - Rooftop feature coverage and distance to roof

Requirement:

If elevator provides access to a rooftop designed to provide usable open space, elevator penthouses and mechanical equipment up to 45 feet above maximum height limit are permitted, provided the structure must be greater than 125 feet in height, and combined total roof coverage of feature gaining additional height does not exceed limits listed in 23.48.025.c.4. At the applicant's option, combined total coverage of all features listed in subsections 23.48.025.c.4 and 23.48.025.c.5 may be increased to 65 percent of the roof area, provided that a. all mechanical equipment is screened; and b. no rooftop features are located closer than 10 feet to the roof edge.

Requests:

1a. Request to extend coverage percentage of total roof features from 65% to 100%, and relief from the 25% limit of total roof coverage of feature gaining additional height.

1b. Request to allow the rooftop features to be located closer than 10 feet to the roof edge.

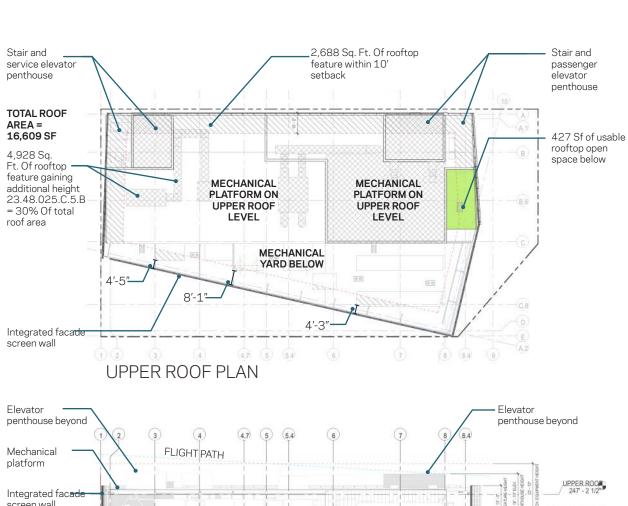
Justification for requests 1a and 1b:

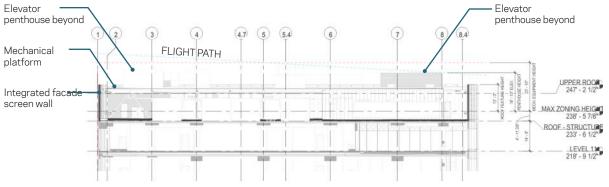
The project's sustainable goals include an energy district system that is more efficient and utilizes heat created from existing city infrastructure but requires more equipment (cs1.a.1 energy choices). which needs to be vertically stacked due to relatively small building footprint. To screen the additional mechanical equipment but also facilitate an integrated facade approach transition from building to sky, the team requests to extend roof top feature coverage from 65% to 100%, and relief from the 25% limit of total roof coverage of feature gaining additional height (dc2.4.i - tall buildings - transition to the sky & skyline composition). The proposed mechanical screen will be integrated into the facade materials and wrapped around the entire perimeter of roof edge at a consistent 13' height above the maximum building height. This facilitates a better transition from building to sky while providing the required screening, allowing the urban read of the building to be continuous and elegant.

Approximately 430 sf of usable open space, accessible by elevator, is provided on the rooftop level. Mechanical equipment extends up to 23'-10" above maximum height limit, as allowed by 23.48.025.C.5.b. Approximately the upper 10'-0" portion of mechanical equipment located on the mechanical platform level will project over the top of mechanical screen.

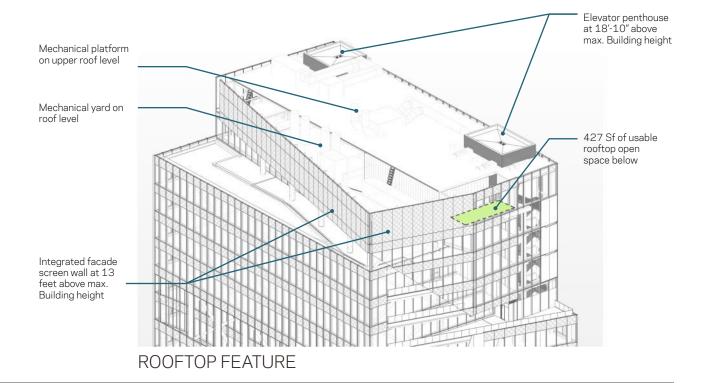
This departure will allow the elevator penthouses and mechanical screen to be integrated into the overall building massing and modulation per design guideline (dc2.4.j - tall buildings transition to the sky & skyline composition) for an intentional, designed terminus to the tall form that strengthens the verticality of the building and enhances the skyline. The enhanced design improves the building's visual interest compared to the code compliant design, and will contribute to the overall skyline profile and variety of forms in South Lake Union. the deliberate extension of the form also enhances the gateway impact of the building

To respond to the context across alley, the elevator and stair penthouses are off-set to the north of the project site to increase the privacy between two sites. Though the core is to the north, egress stairs and elevator vestibules will be clad in some amount of transparent material to maintain activation and interest on facade. The core overrun and the parapet walls will be closer than 10' to the north roof edge. Roof top mechanical penthouse and equipment are also located closer than 10' from the roof edges in varying distance from 0'-0" to 8'-1" (dc2.4.a - tall buildings response to context)



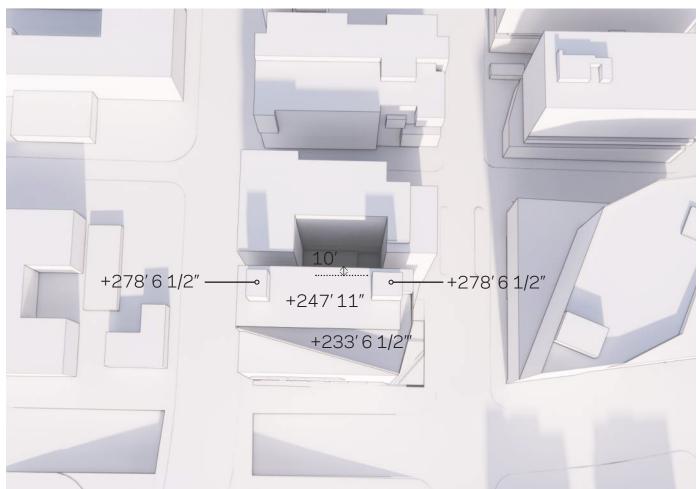


SECTION E-W THROUGH ROOF



#1 shadow study | departures

CODE COMPLIANT



Shadow study - January 21 at noon Roof feature with maximum permitted bulk at 10 feet away from north roof edge

PROPOSED DEPARTURE



Shadow study - January 21 at noon Proposed roof feature with reduced height aligning the north edge LESS SHADOW IMPACT ON NEIGHBORING BUILDING

DEPARTURE #2

23.48.240 B.1b - Except on Class 1 Pedestrian Streets, as shown on Map A for 23.48.240, and as specified in subsection 23.48.240.B.1, the street-facing facade of a structure may be set back up to 12 feet from the street lot line subject to the following (Exhibit B for 23.48.240): 1) The setback area shall be landscaped according to the provisions of subsection 23.48.055.A.3; 2) Additional setbacks are permitted for up to 30 percent of the length of portions of the street-facing facade that are set back from the street lot line, provided that the additional setback is located 20 feet or more from any street corner; and 3) Any required outdoor amenity area, or other required open space, or usable open space provided in accordance with subsections 23.48.240.E, 23.48.240.F, or 23.48.245.B.4.c is not considered part of the setback area and may extend beyond the limit on setbacks from the street lot line that would otherwise apply under subsections 23.48.240.B.1.b.1 or 23.48.240.B.1.b.2.

Request:

Request for setting back the facade of the proposed project more than 12 feet over 30% of lot line along Dexter Ave for an approximate average of 25 feet setback. The additional setback area will be landscaped and located more than 20 feet of street corners.

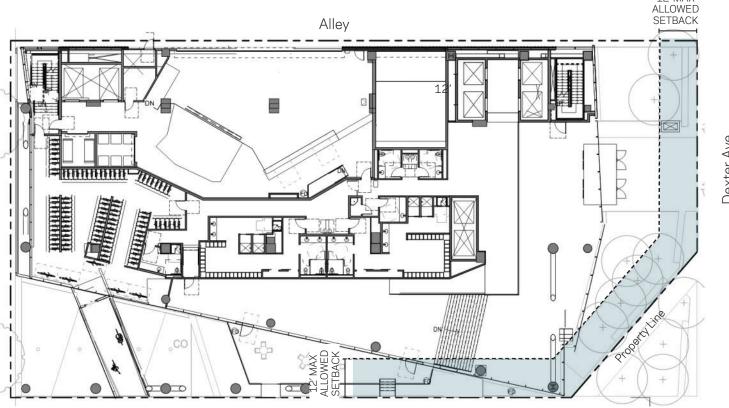
Justification:

The proposed design departure better meets the intent of the design guidelines in making well-scaled, comfortable open space for high pedestrian volumes and activated gateway location. It promotes creative massing and scale consistency with coherent spatial 'street room' at the prominent pedestrian corner (DC2 Street Level Scale). The resulted open space is accessible to the public and visible from the street, while reinforces the relationship between indoor and outdoor spaces on the ground plan (DC3 Building Open Space Relationship). The open plaza creates a comfortable buffer between the proposed building and the busy Mercer-Dexter corner while reinforcing the street corner as an urban gateway. Design elements that will contribute to the gateway: Setbacks, landscaping, facade treatments. (CS2 Gateway locations).

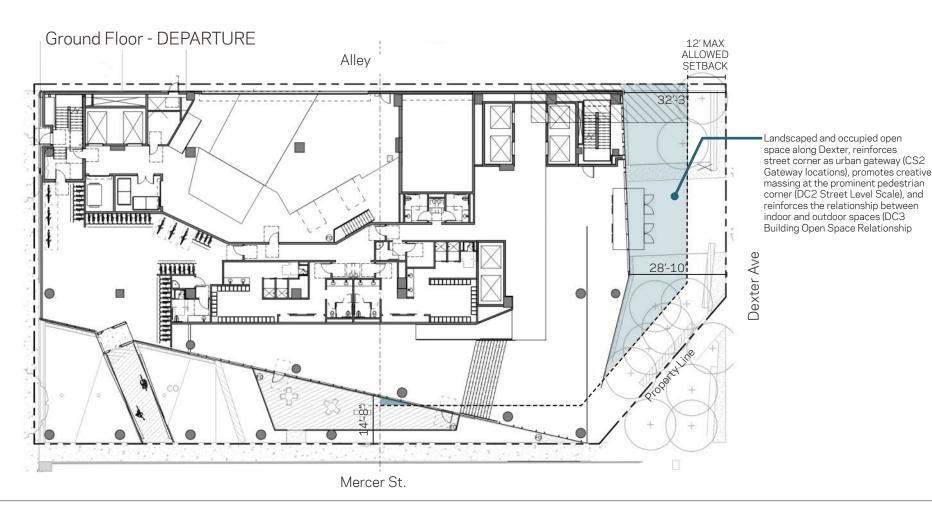
Following design guidelines are enhanced:

- CS2 Urban Pattern & Form 1. Gateway locations
- DC2 Architectural Concept 2.a Street-level scale podium
- DC3 Open Space Concept 1. Building Open Space Relationship

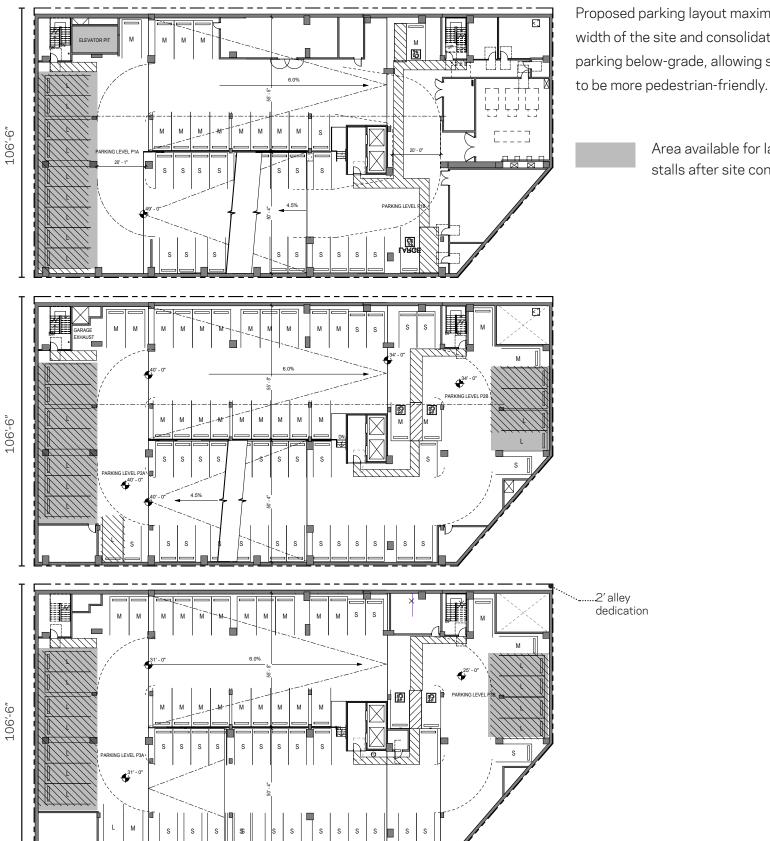




Mercer St.



DEPARTURE #3



Proposed parking layout maximizing the width of the site and consolidating all parking below-grade, allowing street-level

> Area available for large-sized stalls after site constraints

23.54.030.B.2c - Parking requirement, non-residential

Requirement:

When 20 or more parking spaces are provided, a minimum of 35 percent of the parking spaces shall be striped for small vehicles. The minimum required size for small parking spaces shall also be the maximum size. A maximum of 65 percent of the parking spaces may be striped for small vehicles. A minimum of 35 percent of the spaces shall be striped for large vehicles

Request:

Request for reducing the percentage of large vehicles spaces from 35% to 20%

Justification:

The proposed parking design maximizes efficiency of the garage and the ability to create a great pedestrian environment by efficiently consolidating all parking in a below-grade garage. (CS1 - Topography and elevation change) The project site has a clear width of about 106'-8" in north and south direction, after alley dedication, shoring walls, and basement walls have been accounted for. The dimension is not wide enough for large vehicles parking spaces with the required drive aisle width per zoning requirement. It resulted in a parking layout of that maximizes large vehicles spaces at the East-West ends where spaces are comparatively flexible. The resulting percentage of the maximized large vehicles spaces is 20% per proposed layout.

The ground level becomes more safe by the proposed efficient belowgrade parking which facilitates convenient access points for all modes of travel (PL4 - Active Transportation) and allows network of open spaces to happen around the project site positively contributing to the neighborhood. (PL1 - Enhancing Open Space).

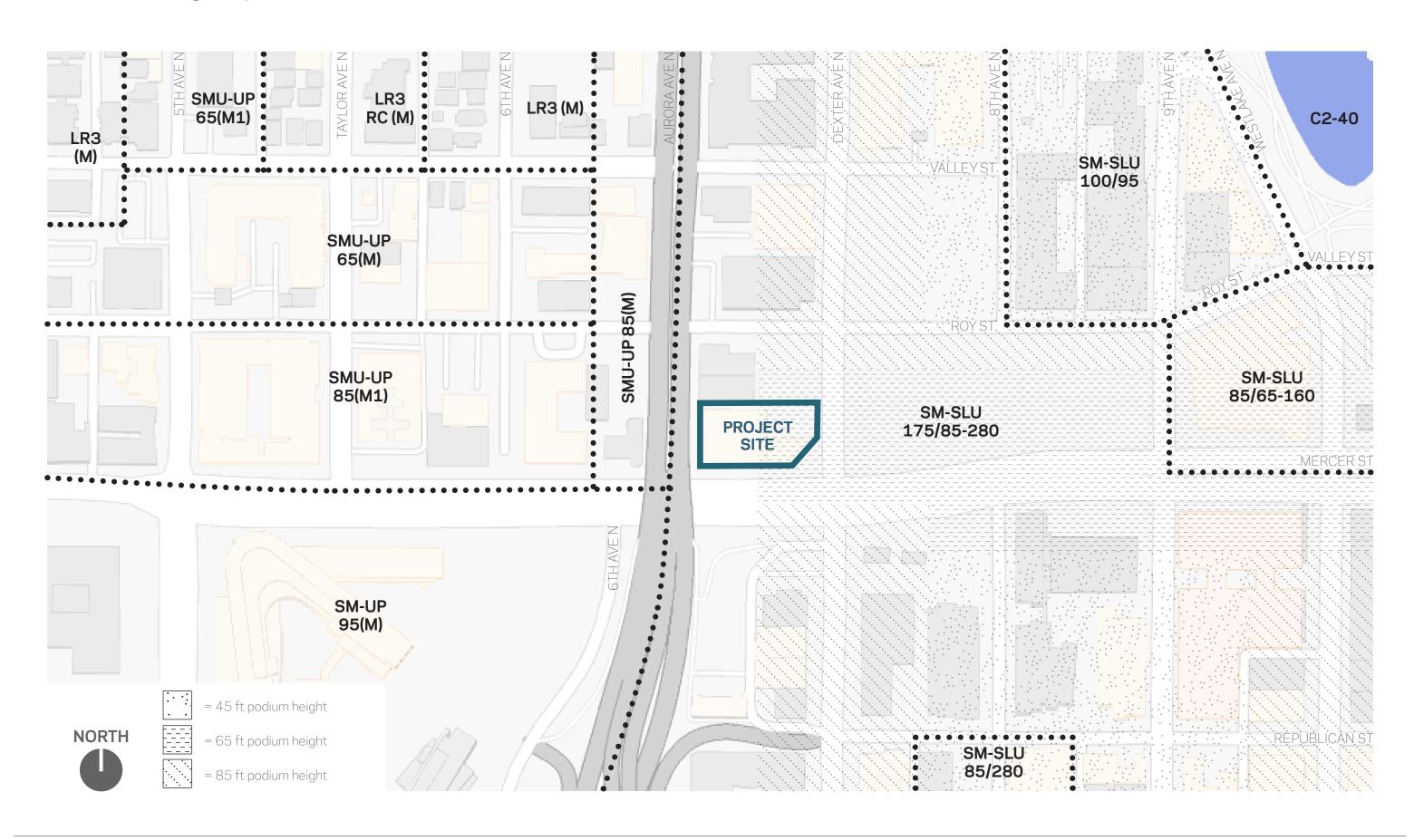
Following design guidelines are enhanced:

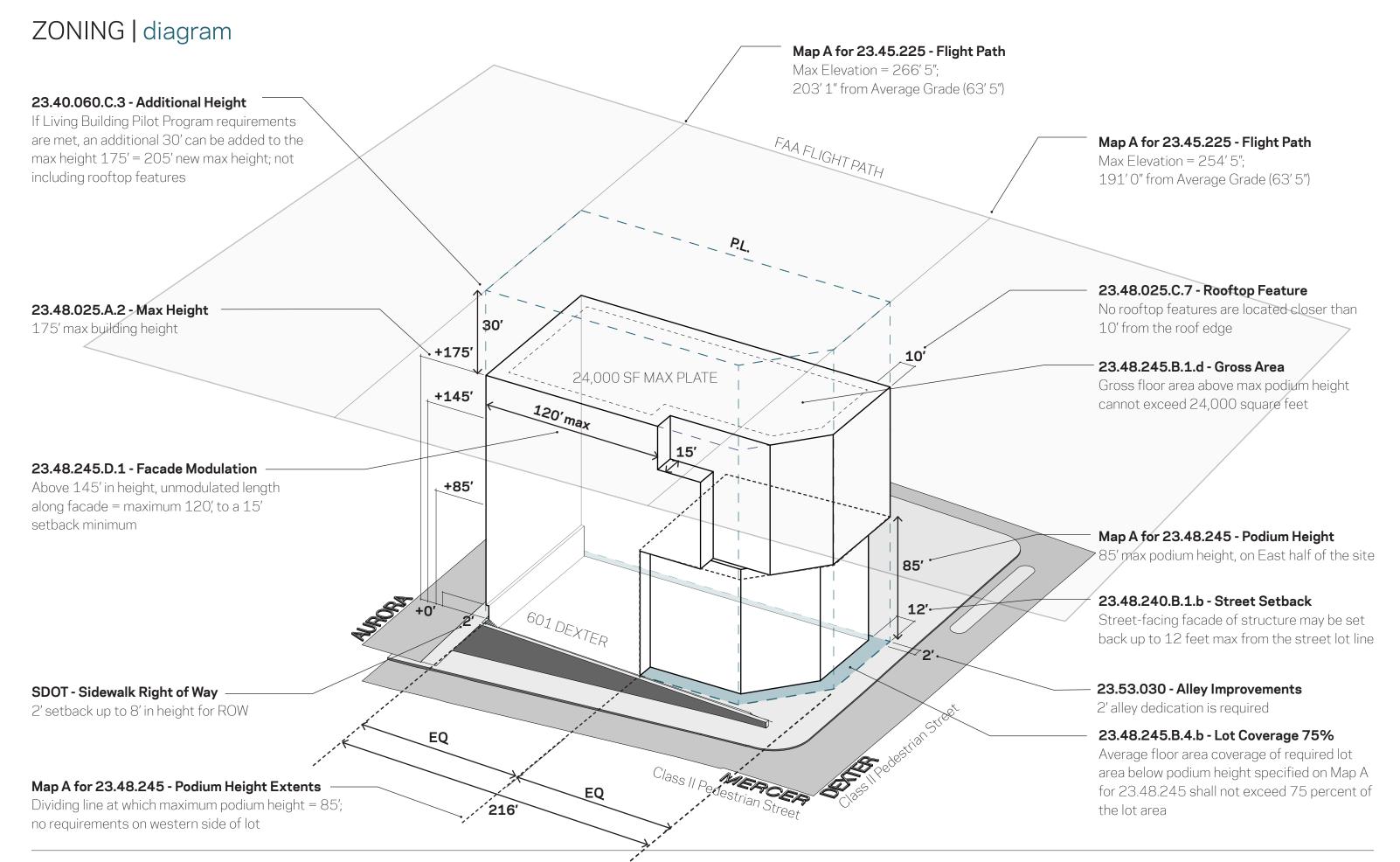
- CS1 Topography and elevation change 3.c. Conceal underground parking
- PL4 Active Transportation -A.1 Serving all modes of travel
- PL1 Connectivity -A.1 Enhancing Open Space
- DC1-A Arrangement of Interior Uses

APPENDIX A



ZONING | zoning map





ZONING | land use code summary

Code:

Seattle Municipal Code, Title 23, Chapter 48, Seattle Mixed

Address:

601 Dexter Ave North, WA 98109

Zone:

SM-SLU 175/85-280

Overlay:

South Lake Union Urban Center

23.48.005 Uses

A. Uses: Offices / Laboratory uses permitted

D. No required street uses; No Class I pedestrian streets on site (Map A 23.48.240)

23.48.025 Structure Height

A.2 Max Height limit (non-residential): 175'

C.4 Rooftop features may extend up to 15 ft above max height limit allowed

C.7 No rooftop features are located closer than 10 feet to the roof edge. Roof coverage may be increased to 65 percent.

23.48.040 Street-level Development Standards

A.2.b On Class 2 Pedestrian Streets, minimum height for street facing facades is 25 feet: Dexter Ave N, Mercer St

A.2.c On all other streets, the minimum height for street-facing facades is 15 feet: Aurora Ave N

B.1.b On Class 2 Pedestrian Streets, minimum of 60 percent of street-facing facade must be transparent; Blank facades are limited to segments 15 feet wide: Dexter Ave N, Mercer St

B.1.c On all other streets, minimum 30 percent of street-facing facade must be transparent; blank facades are limited to segments 30 feet wide: Aurora Ave N

C.1 No street level uses required, any street level uses exempt from FAR shall meet the following:

C.2 No minimum frontage requirement for street level uses provided

C.3 Minimum floor-to-floor height of 13 feet and extend at least 30 feet in depth

C.5 Street-level uses shall be located within 10 feet of the street lot line.

23.48.055 Landscaping and Screening

A.2 Landscaping that achieves a Green Factor score of .30 or greater, pursuant to Section 23.86.019, is required

23.48.080 Required Parking and Loading

A Off-street parking spaces and bicycle parking are required according to Section 23.54.015

B. Loading berths shall be provided pursuant to Section 23.54.035

23.48.085 Parking & loading location, access, and curb cuts

D.1 Parking access will be from the alley

23.48.220 FAR

A. FAR limits for non-residential uses: Base FAR = 4.5 Maximum FAR = 8

23.48.221 Extra FAR

A.2. Means to achieve 75 percent of extra non-residential floor area on the lot by using bonus non-residential floor area for affordable housing and child care pursuant to Section 23.58A.024. Means to achieve 25 percent of the extra non-residential floor area include Regional Development Credits.

C. Minimum requirement. Applicant shall make a commitment that the Proposed development will meet the green building standard.

23.48.223 Mandatory housing affordability (MHA) Program

Provisions of chapters 23.58B apply to the project.

23.48.225 Structure Height

A.1 Max Height limit (non-residential): 175'

E. The proposed structure height will not physically obstruct use of the flight path shown on Map A for 23.48.223 or endanger aircraft operations.

23.48.230 Additional height in certain SM-zoned areas

The proposed structure (Baseline option) does not exceed allowable non-residential height limit of 175 ft.

23.48.240 Street-level Development Standards

B.1.a No Class 1 pedestrian streets abutting the site.

B.1.b Street-facing facade may be set back up to 12 ft from street lot line.
B.1.b.3 Any required outdoor amenity area or other required open space is not considered part of the setback area and may extend beyond the limit on setbacks from the street lot line

23.48.245 Upper-level Development

B.1.d Max gross floor area above podium height is 24,000 sq ft per story

B.4.a Podium height is 85' on the east half of the project site per Map A for 23.48.245

B.4.b Average floor area coverage of required lot area below podium height specified on Map A shall not exceed 75 percent of the lot area

D.1 The maximum length of a facade without modulation for stories above podium height per Map A for 23.48.245 up to 145 feet is 150 feet in length; for stories above 145 feet the maximum unmodulated facade length within 15 feet of street lot line is 120 feet

D.2 If a portion of a facade that is within 15 feet of the street lot line is the maximum length permitted for an unmodulated facade, the length of the facade may be increased only if additional portions of the facade are set back a minimum of 15 feet from the street lot line for a minimum distance of 40 feet.

23.48.250 Open Space Requirement

Quantity of open space in the amount of 20 square feet for each 1,000 square feet of gross office floor area is required.

23.48.280 Required Parking

B. Maximum parking limits the amount of parking reserved for accessory to non-residential uses is limited to one parking space per every 1,000 sq ft of gross floor area in non-residential uses minus gross floor area in parking.

23.54.015 Required Parking

The minimum number of off-street spaces for bicycles required for specific uses is set forth in Table D for 23.54.015. = A.5 Offices and laboratories, research and development: 1 per 2,000 square feet (long-term), 1 per 10,000 square feet (short term)

.8. Structures containing 100,000 sq ft or more of office use floor area shall include shower facilities and clothing storage areas for bicycle commuters. Two showers shall be required for every 100,000 sq ft of office use.

23.40.060 Living Building Pilot Program

B.1 The project meets ILFI Living Building Challenge Petal certification by attaining at least three of the seven performance areas, or "Petals" including at least one of the following three petals: Water, Energy, or Materials

B.2 Total annual building energy use that is 25 percent less than a baseline defined as the EUI targets in the Target Performance Path of Seattle Energy Code Section C401.3

B.3 None of the space heating and water heating gin the project uses on-site combustion of fossil fuel

B.4 The project uses only non-potable water to meet the demand for toilet and urinal flushing, irrigation, hose bib, cooling tower (make up water only), and water features

C.1.a. Twenty-five percent more gross floor area than the otherwise applicable maximum floor area ratio

A project qualifying for the Living Building Pilot Program may employ additional structure height = 30 additional feet for zones with height limits greater than 85 feet and non-residential use

As an allowance for mechanical equipment fully contained within the structure related to participation in the pilot, one-half percent shall be deducted in computing gross floor area subject to floor area limits in addition to any allowance for mechanical equipment included in the applicable zone

23.53.030 Alley Improvements

D.1 Per Table C, 2 feet alley dedication is required.



MAP A 23.48.245 Podium Heights

45 feet

65 feet

85 feet

CS 1 Natural Systems & Site Features

CS 1.3 Topography and Elevation Changes

Accommodate sloping terrain through 'stepping' ground floor and other architectural features. Emphasis should be placed on ground-level treatments that create a safe, attractive transition between the site and pedestrian zone.

b. Setback or recess entrances for a gracious transition from a sloped sidewalk to a flat grade at the entry.

c. Conceal underground parking from street views and design any parking walls exposed above grade-level with attractive treatment such as integrated, quality architectural cladding, planting and/or artwork.

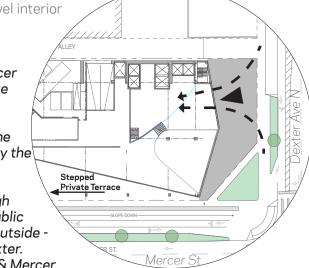
d. Create a safe visual transition between ground-level interior

and adjacent pedestrian areas and public sidewalks.

ARCHITECT'S RESPONSE:

The primary entry, from the gateway corner of Mercer & Dexter, is recessed to clearly define and articulate the main point of arrival, as well as provide a substantial transition area from the street to lobby entry. This street-level setback (colored in pink to the right) provides ~2812sf of open space accessible by the public off of Dexter.

All underground parking walls are concealed through grading and landscape to maintain an attractive public realm. The ground floor - and extended landscape outside - follows the sloping terrain from Aurora down to Dexter. Finally, a high degree of transparency from Dexter & Mercer provides visual cues to arrival & public areas.



Ground Level Plan

CS 1.4 Plants and Habitat

South Lake Union is on a bird and insect flight path between greenbelts on Capitol Hill, Queen Anne, and Magnolia.

a. Consult with landscape architects to develop landscape plans that provide refuge habitat and food sources in project landscape species to facilitate movement for urban populations of some species.

b. In designing open spaces, Green Factor measures, green roofs, and other landscape element consideration should be given to plantings and other elements (such as fountains) that might be used by such species.

ARCHITECT'S RESPONSE:

Primary landscape features, particularly at the gateway corner of Mercer & Dexter, are designed to re-generate the local ecology. The substantial ground-level landscaping will take advantage of existing topography to filter water runoff and re-build the local ecology. Additional opportunities for landscaping include the roof, southwestern deck, and northeastern deck.



Landscape Concept Sketch

CS 2 Urban Pattern & Form

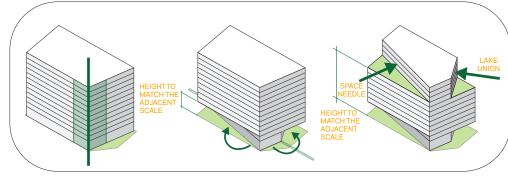
CS 2.1 Gateway Locations

The South Lake Union Urban Design Framework (UDF) identifies important gateways to consider in project design. Gateways are transition locations and places that mark entry or departure points to the neighborhood for automobiles and pedestrians. Private sites at gateways should create opportunities for identification - a physical marker so the community notices they are entering a special place.

- **a.** Consider site characteristics such as topography, views, or surrounding building patterns, which are important for gateway locations.
- **b.** Gateway locations: design elements that contribute to gateways include building out to meet the corner where appropriate, or tools such as: setbacks to allow pedestrian friendly spaces and expanded sidewalks; signage; landscaping; artwork; signature facade treatments.

ARCHITECT'S RESPONSE:

The site sits at the middle ground where Mercer circulation drops in elevation while site topography slopes up towards the elevated Aurora. The proposed massing creates a continuous vertical line at the gateway corner that begins to respond to the scale of this elevation difference. Serving as a "pin," this line anchors the other massing responses, holding the volumetric gateway at Dexter and Mercer.



Gateway Pin

Inviting Setback Angles

Neighborhood Signifier

The massing then forms setback angles around the "pin", inviting circulation from all directions on Mercer and Dexter. These setbacks provide opportunities for landscaping and artwork. This overall massing, combined with amplifying the high ground through the ground plane, marks this gateway location.

Moreover, the preferred massing intentionally differentiates itself from the surrounding more rectilinear building patterns (while also relating to the changes in contextual scale) in order to create a building-scale, three-dimensional physical marker signifying the neighborhood transitions for the community.

Adjacent Streets

Project design should respond to adjacent street character. These street descriptions should inform how projects relate to the right-of-way. Specific design guidance for projects on specific streets below.

a. Aurora and Dexter Avenues N: New development should help to make these arterial entries into downtown by implementing substantial landscaping and attractive building facades. Balconies and outdoor living spaces are less desirable facing these streets. Because these arterials will be primarily experienced by vehicle, the scale of street improvements and facade elements could be larger than if these streets were predominantly pedestrian-oriented.
h. Mercer Street: Mercer Street is the widest and most heavily used street in South Lake Union. Strong street walls on both sides of the street will enhance the street's spatial characteristics. Ground floors should contain active building uses such as lobbies and group work spaces facing the corridor as well as retail and other pedestrian oriented uses. Ground floor spaces should be lit at night. Of special note is the "Teardrop Site" at Broad Street, which provides opportunities for a number of special uses and activities.

ARCHITECT'S RESPONSE:

The setback from Dexter Avenue on the ground floor provides a substantial landscape area within the public realm, while still providing the necessary urban edge to respond to the Boulevard/Great Street classification in the massing above. Similarly, opening a landscaped area at the corner of Mercer and Aurora creates visual relief while pushing the massing above to the boundary along Aurora responds to the higher-paced traffic.

The context along Mercer is unique, at a higher elevation than the majority of pedestrian circulation. Landscaping and an intentionally exposed structure provides an intriguing street presence at Mercer, but majority circulation / activation is engaged at the corner of Mercer and Dexter via a work lounge, coffee lounge, landscaping, and bike facilities.

PL 1 Connectivity

PL 1.1 Network of Open Spaces

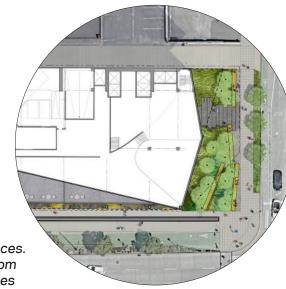
Open spaces in South Lake Union include mid-block connections, ground-level open space developed in new projects, and three parks: Denny Park, Cascade Playground, and Lake Union Park. Including green streets, Class I Pedestrian streets, and development of an open space network is a priority for the neighborhood. These spaces play a critical role in the transportation system and provide space for community activity.

Because of the importance of mid-block connections and of the open space connectivity within a network, these features should be designed as high priority amenities when granting departures from development standards. Proponents should consider the following:

- **b. Street-Level Open Space:** For both retail and residential focus areas, consider private or semi-private courtyards facing the street, or pocket parks.
- **c. Open Space Connections:** Open space connections should respond to view corridors of neighborhood-scale and regional open spaces, such as the Seattle Center, Lake Union, Denny Park, and Cascade Playground.

ARCHITECT'S RESPONSE:

The preferred massing is generally defined by 3 major contextual responses, all of which engage the neighborhood network of open spaces. The southwest upper level terrace provides expansive views to and from the Seattle Center. Along Dexter, a northeast mid-level terrace provides a view to and from Lake Union.



Linking Elliot Bay, Seattle Center, & Lake Union

Finally, at the ground level, a Entry Lobby and Work Lounge is located right at the gateway corner, visually activating both double height open spaces along Mercer and Dexter, primary access corridors between Seattle Center and Lake Union. The landscape along Mercer will be a private courtyard / elevated terrace while the one along Dexter will be a publically accessible open space with landscaped DC2.6 amenities.

PL 4 Active Transportation

PL4.1 Bicycle Facilities

Bicycle use and parking should be encouraged to promote a healthy and active neighborhood and to support local businesses. Bicycle racks should be plentiful, and either be from the Seattle Department of Transportation's bike parking program, or be an approved rack of similar 'inverted U' or 'staple' style. The bicycle racks may also be an opportunity for placemaking, such as having a uniform color for bike racks within South lake Union or having distinctive place-names designed into the racks.

ARCHITECT'S RESPONSE:

Adjacent to the Mercer and Dexter bike corridors, the ground floor design provides easy and accessible bike storage and locker room facilities. A separate entrance on the ground floor off of the alley, independent of the main lobby, ensures a high degree of accessibility.

The increased landscaped setback on Dexter is adjacent to a protected bike lane. That area will also provide temporary public bike racks, further facilitating bike transportation up and down Dexter.

DC 2 Architectural Concept

DC 2.1 Massing, Design and Scale

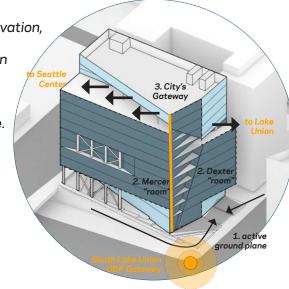
Consideration of three scales. Buildings and their surroundings are perceived at three scales:

- 1. The pedestrian scale that relates to human activity within the immediate vicinity of the pedestrian (roughly 60' horizontally),
- 2. The street space where the street and adjacent open spaces are perceived as a "room" (generally street block or two long and about 60' high); and
- 3. Tall building or skyline scale (where the building form is perceived generally at more than a block away).

Considerations for the pedestrian scale are discussed in preceding section PL1, 2, and 3. Articulation of podiums is discussed DC2.3 on page 21, and the zoning code limits podium heights in some areas to reduce the scale of a block front. Design concerns for tall buildings are discussed in DC2.4 on page 22.

ARCHITECT'S RESPONSE:

A recessed entry and rich, textured ground plane provide activation, legibility, and scale for the pedestrian. Seating elements and landscape features create additional layers. The massing then responds to the street, creating a podium datum and framing the "room" of Dexter, and maintaining the street edge along Mercer. Lastly, the Gateway "Pin" gives a sense of grandeur and scale to the Gateway Corner, and a more dynamic skyline.



Scale and Texture

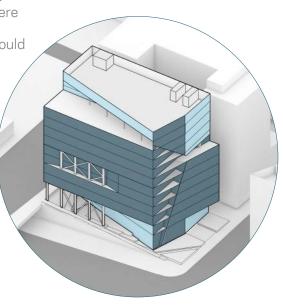
a. Texture: Materials such as brick, stone, precast concrete, smaller paned glass, tile, etc. provide both scale and texture and should be selected, especially where the surfaces are prominent or where there are no other architectural features. For example, where structured parking is exposed because of site topography, the 'blank walls' should be textures with high quality materials.

ARCHITECT'S RESPONSE:

The proposed massing is defined by 3 contextual responses, addressing context at the macro-urban, the street "room," and pedestrian scales. These responses deflect from the prevailing street edge, creating a duality of conditions.

The larger scale, urban elements hold the "perimeter envelope" of the massing, and the finer more human scale elements begin to feature themselves at the entrances and landscape spaces - engaging with the pedestrian experience. High quality materials with high transparency will be provided at these street levels to facilitate interaction with pedestrians at the ground plane. Other materiality will be used to accentuate the distinct characters.

3 Scales



Texture & Differentiation

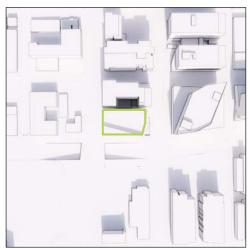
SITE | shadow study

March 21st

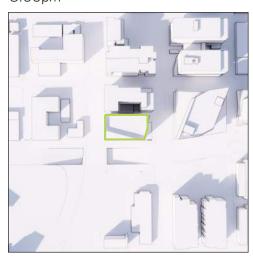
9:00am



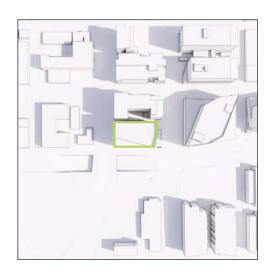
12:00pm



3:00pm

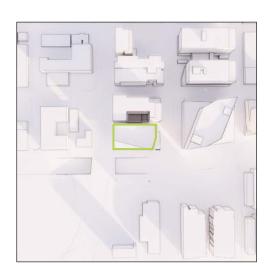


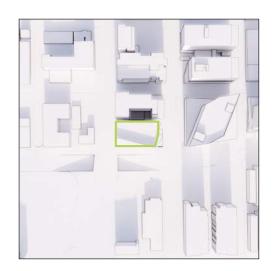
June 21st





December 21st







SITE | site survey

Parcel a:

Lots 7 and 8, and the west 25 feet of lots 1 and 2 in block 3 of eden addition to the city of Seattle, as per plat recorded in volume 1 of plats, page 61a, records of king county;

Except that portion of said lots 2 and 7, conveyed to the city of Seattle for alley purposes by deed recorded under recording number 330675;

Also except that portion of said lots 7 and 8 condemned in king county superior court cause number 236360 for aurora avenue, as provided for by ordinance number 59719 of the city of Seattle. Situate in the city of Seattle, county of king, state of Washington.

Parcel b:

Portions of lots 1 and 2 in block 3 of eden addition to the city of Seattle, as per plat recorded in volume 1 of plats, page 61a, records of king county, described as follows:

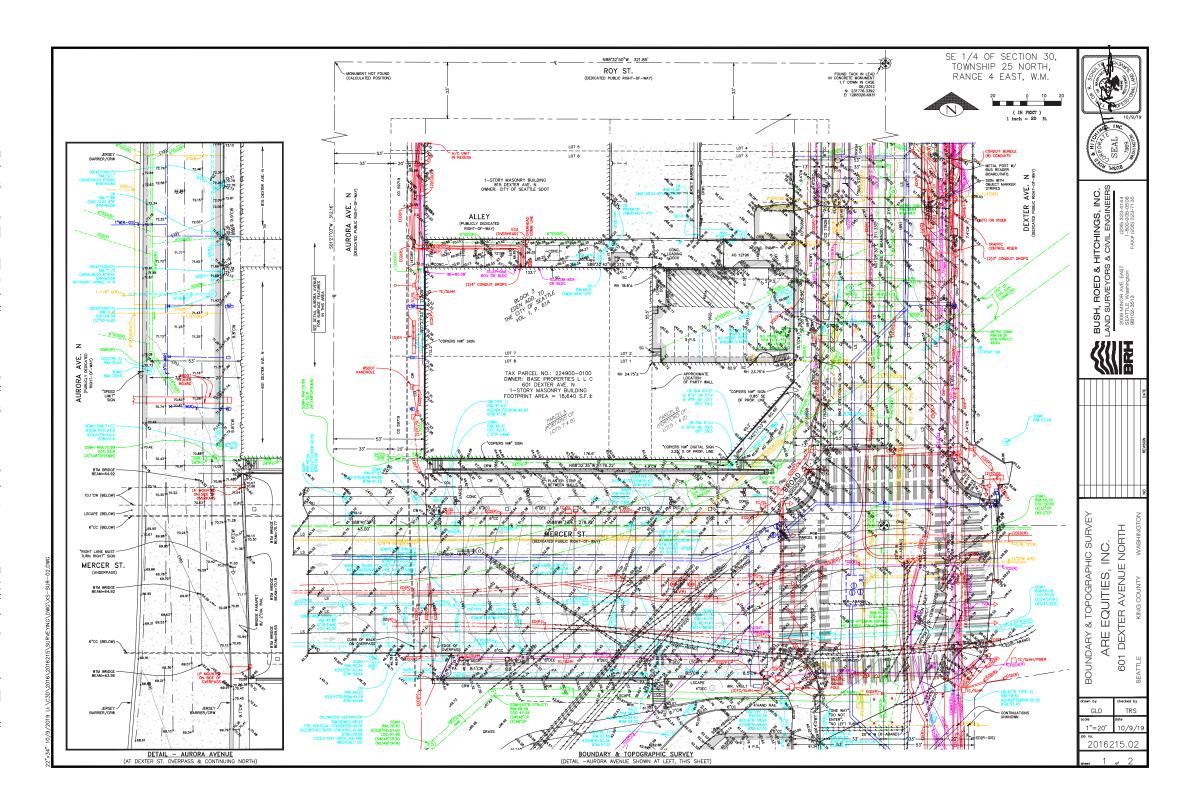
Beginning at the southeast corner of said block 3;

Thence north along the east line of said block 112 feet, more or less, to the south line of alley conveyed to city of Seattle by deed recorded under recording number 330675, and accepted by ordinance number 12196 of said city;

Thence west 103 feet;

Thence south 112 feet, more or less, to the south line of said block; thence east 103 feet to the point of beginning; except portion conveyed to the city of Seattle for street purposes by deed recorded under recording number 1178518, accepted by ordinance number 38011;

And except portion lying east of the west line of dexter avenue as said avenue was widened and established by condemnation decree in cause number 193437, pursuant to ordinance number 50890 of the city of Seattle; situate in the city of Seattle, county of king, state of Washington.



SITE | plan

Information

Address:

601 Dexter Ave N, Seattle WA 98109

Owner's name:

ARE-SEATTLE NO 32 HOLDING LLC

Legal description:

EDEN ADD LESS ALLEY LESS ST

Parcel number:

224900-0100

Site area: 23,282 sq.ft.

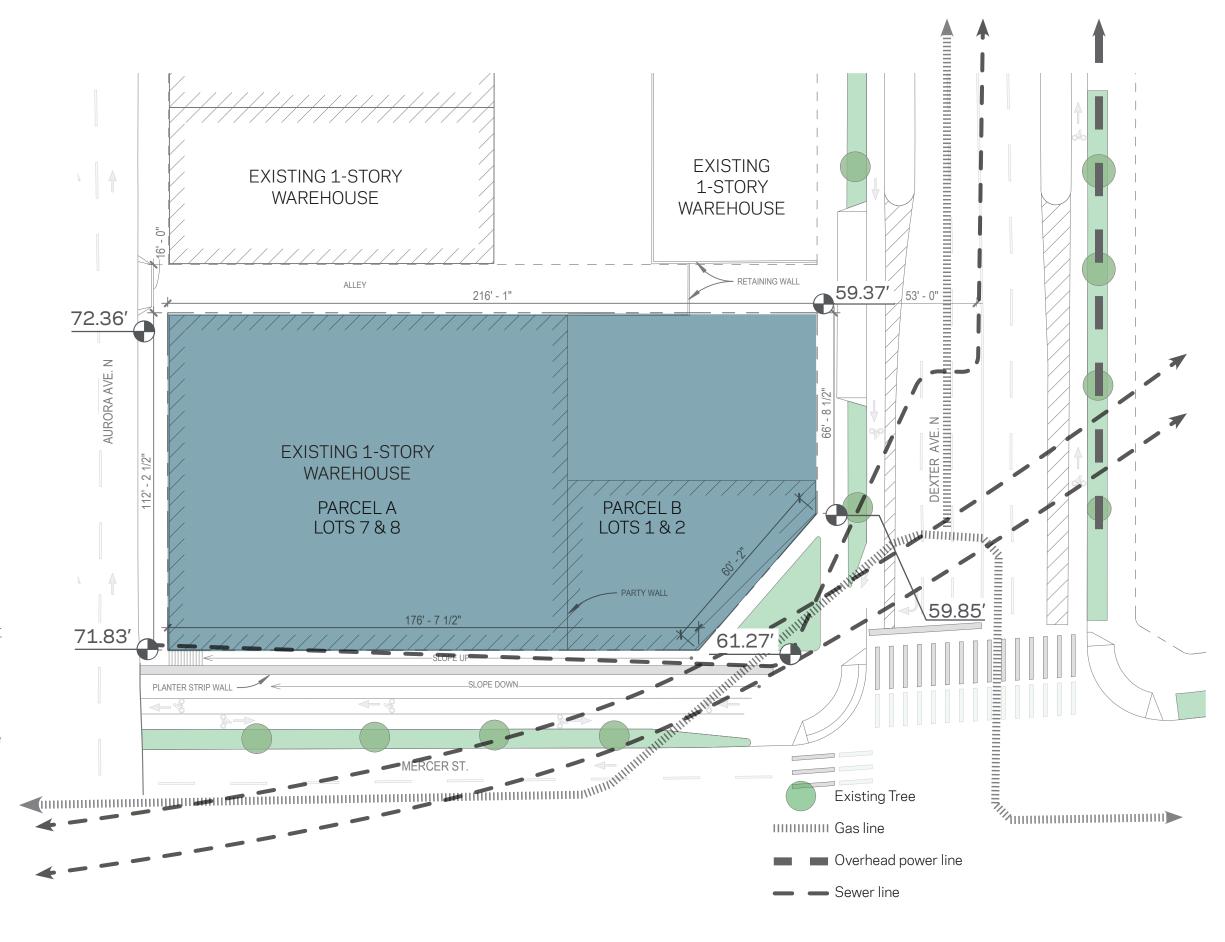
Zoning: SM - SLU 175/85 - 280

Max. FAR: 8.0

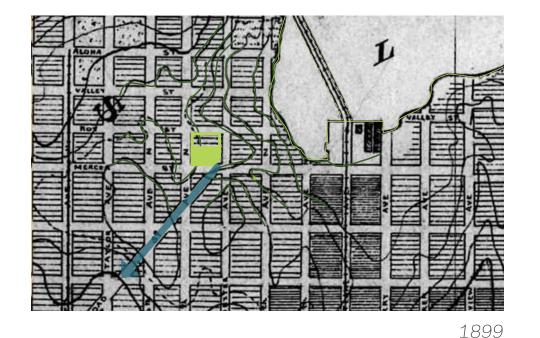
Max Height: 175'

Existing Conditions

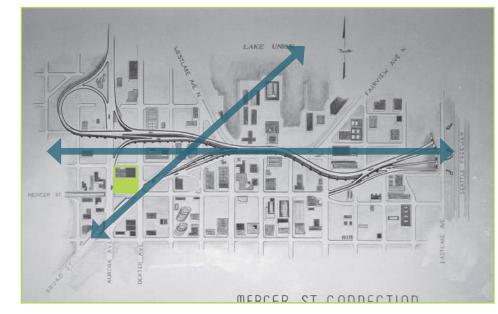
- Vehicular access to site is from Dexter Ave N and through the alley on Aurora Ave N.
 Alley that is accessible through Aurora is not currently connected to Dexter Ave N.
- Protected bike lane starts on the Mercer Ave N.
- Activity is pushed towards Mercer by providing protected bike lane and accessible pedestrian sidewalk.
- Sidewalk to the south of the property connecting Dexter Ave N to Aurora Ave N is not an accessible means of connection.



CONTEXT | neighborhood history



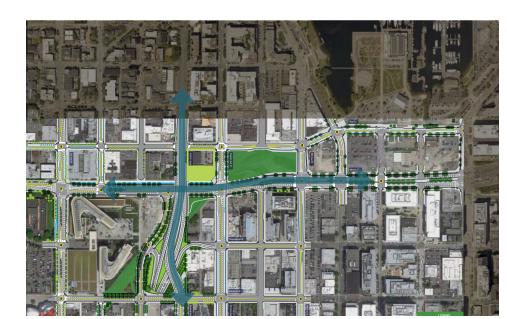




Bay Freeway - 1960s







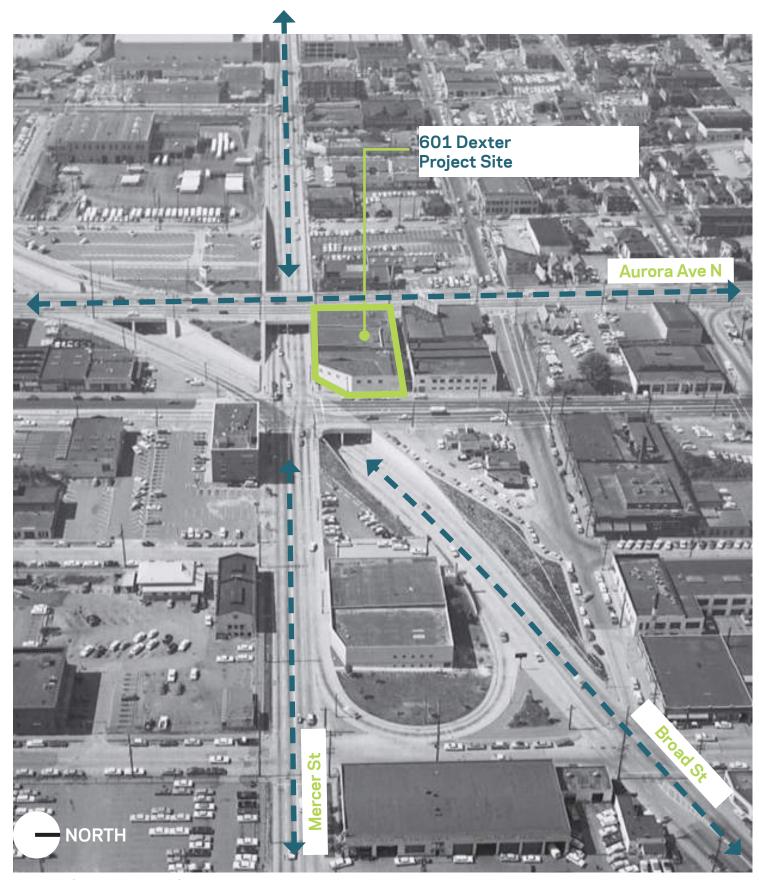
Seattle Commons Draft Plan - 1993

Mercer Corridor Plan - 2015

The site has a history of being at the center of urban aspirations to connect Seattle to Lake Union, the east to the west, and the north to the south, defining the project site boundaries (especially the chamfered southeast corner) as early as 1936.



CONTEXT | neighborhood history



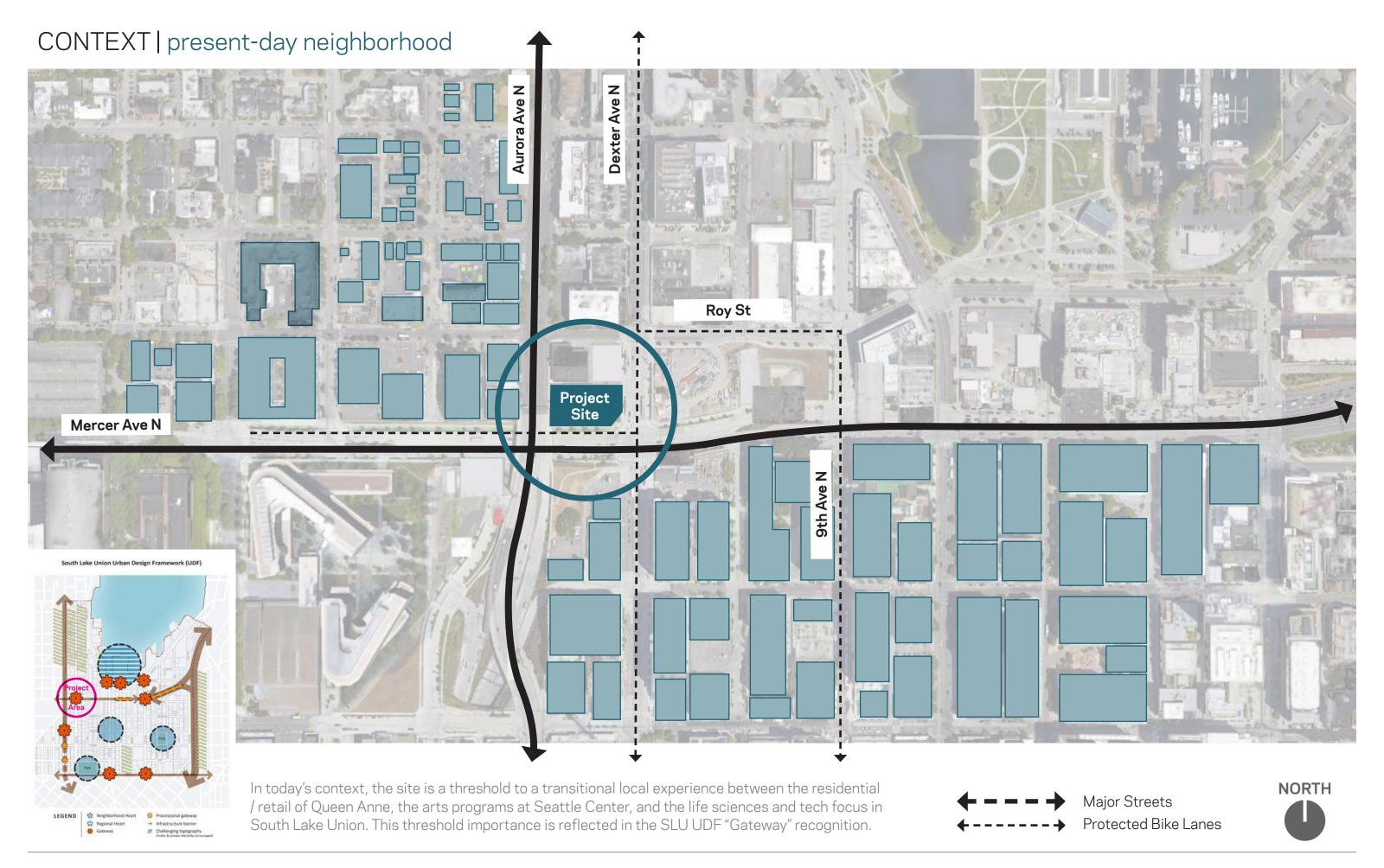
Aerial of Queen Anne featuring Broad St and Mercer Ave - 1962







Denny Regrade captured in three decades show Seattle's volumetric shifts



CONTEXT | program and scale

BILL & MELINDA GATES FOUNDATION

ALLEN **INSTITUTE**

UW MEDICINE RESEARCH FACILITY FRED HUTCH CANCER RESEARCH CENTER

PRESAGE BIOSCIENCES + NANOSTRING









SCIENCE















CASCADE PUBLIC MEDIA

MAD ART

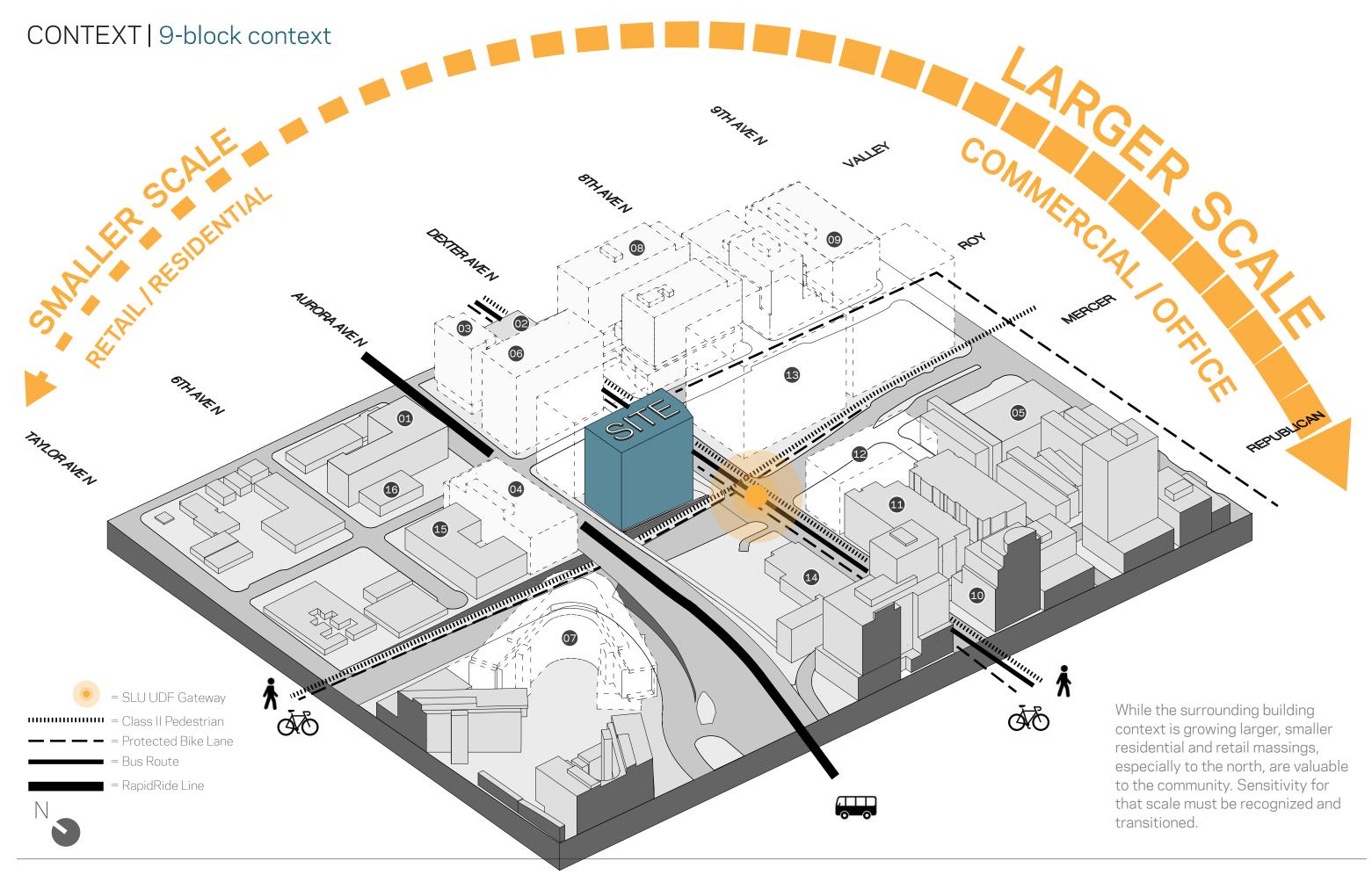
SEATTLE CENTER

SEATTLE REPERATORY THEATER CORNISH PLAYHOUSE

PACIFIC NORTHWEST BALLET

MCCAW HALL / SEATTLE OPERA

A programmatic shift... The site is also at the intersection and boundary of civic performing arts in Lower Queen Anne and the lab / science / research in South Lake Union - one of the main reasons for movement through the gateway intersection.



CONTEXT | 9-block context

The site's 9-block context has been under a lot of change, and based on updated zoning guidelines and submitted SDCI projects, looks to continue evolving its character in the next decade.

Historically (and still presently to the site's north and west), the context included multi-family residential and smaller retail buildings between low to mid-rise heights.

Presently (represented by newer built buildings and submitted SDCI projects to the site's south and east), the context is growing rapidly, becoming a strong science and tech office neighborhood with a taller mid-rise presence and more perimeter glazing to accommodate the height.

Nearby Facade Examples





Mixed-Use / Multi Family

mid-rise residential buildings (~100'- 160') with panelized orthogonal massing made up of lot line extrusion, balconies, and inset boxes

Office / Laboratory / Research

mid-rise office and laboratory buildings (~100'-160') with majority orthogonal, stacked massing that maximize site coverage with high performance glazing systems, a lot of which are floor-to-ceiling

Commercial /

low-rise retail and small commercial buildings (~24'-50' in height) with large adjacent surface parking and small orthogonal

Retail

footprints



Stream Uptown Apartments 708 6th Åve N 6-Story Multi Family



Oakwood Seattle Apartments 717 Dexter Ave N 6-Story Mixed Use / Multi Family



UW Medicine Research Facility 750-850 Republican St 5 Story Lab / Office



820 Roy St 7-Story Office w/ Retail





400 Dexter Ave N 11-Story Office w/ Retail



601 Roy St



SDCI #3017871 701 Valley St 15-Story Multi Family



500 5th Ave N 5-Story Office



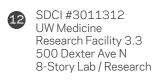
500 Dexter Ave N 8-Story Lab / Office



601 Aurora Ave N 8-Story Hotel / Multi Family



700 Dexter Ave N 14-Story 2-Tower Office w/ Retail







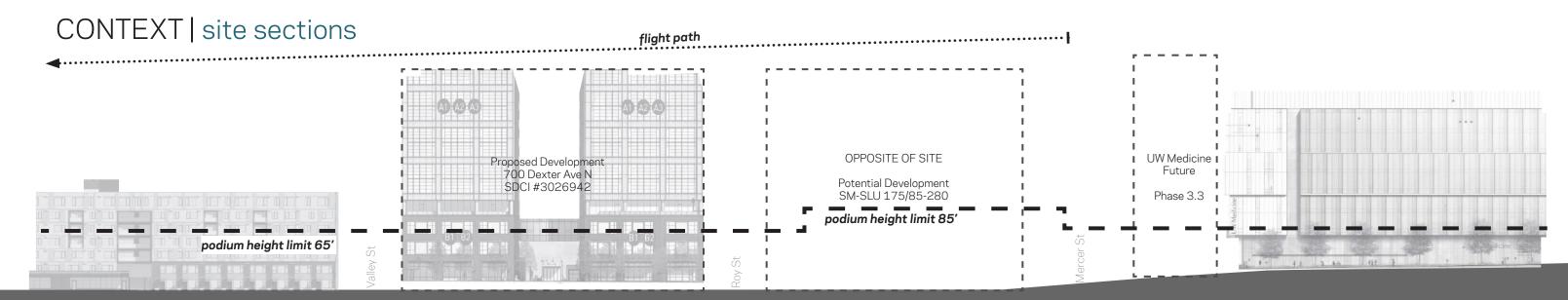
Seattle Strength and Performance 525 Dexter Ave N 1-Story Retail



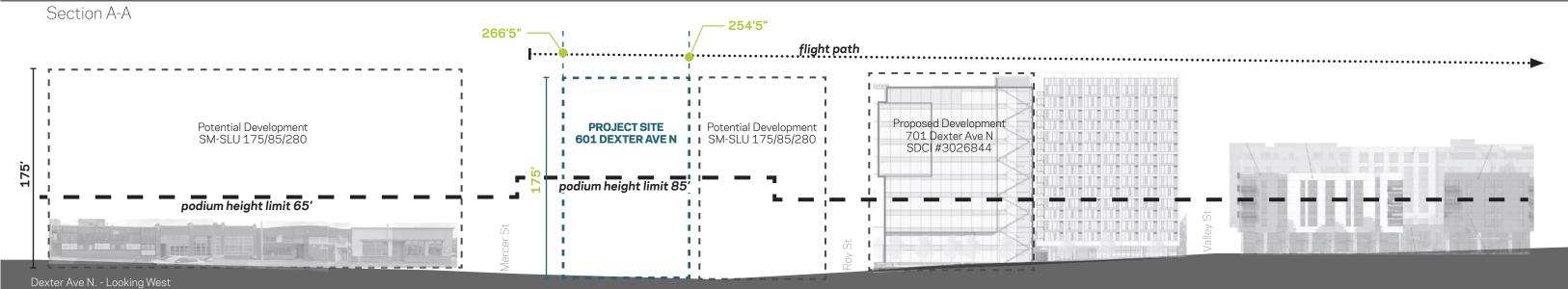
Four Points Sheraton Hotel 4-Story Commercial

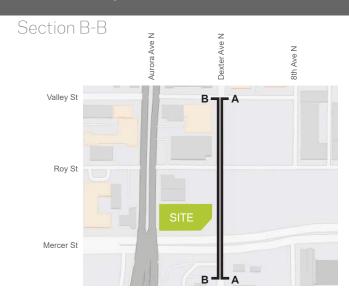


Downtown Automotive Repair 702 6th Ave N 1-Story Commercial



Dexter Ave N. - Looking East

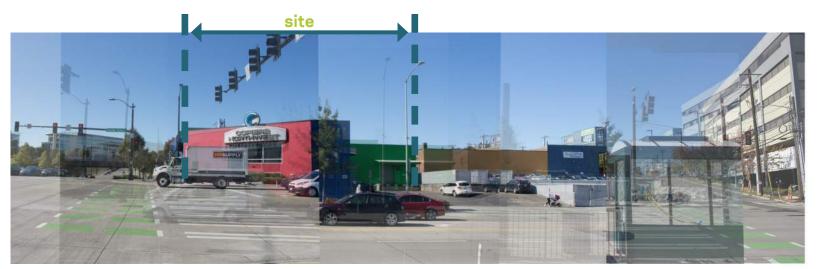




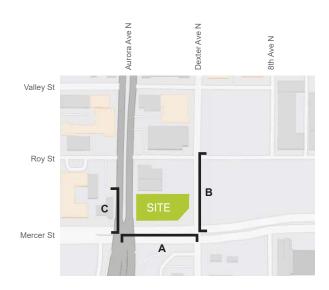
CONTEXT | site elevations

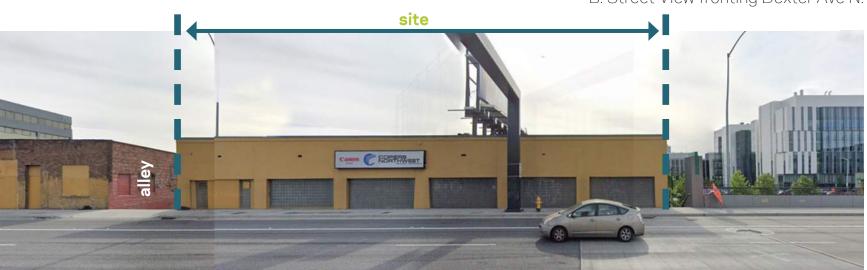


A. Street View fronting Mercer St.



B. Street View fronting Dexter Ave N.





C. Street View fronting Aurora Ave N.

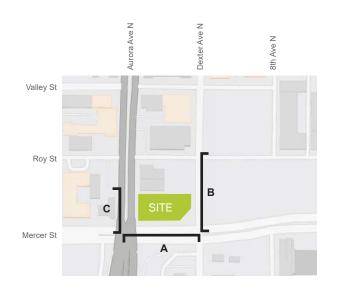
CONTEXT | site elevations



A. Opp Street View fronting Mercer St.

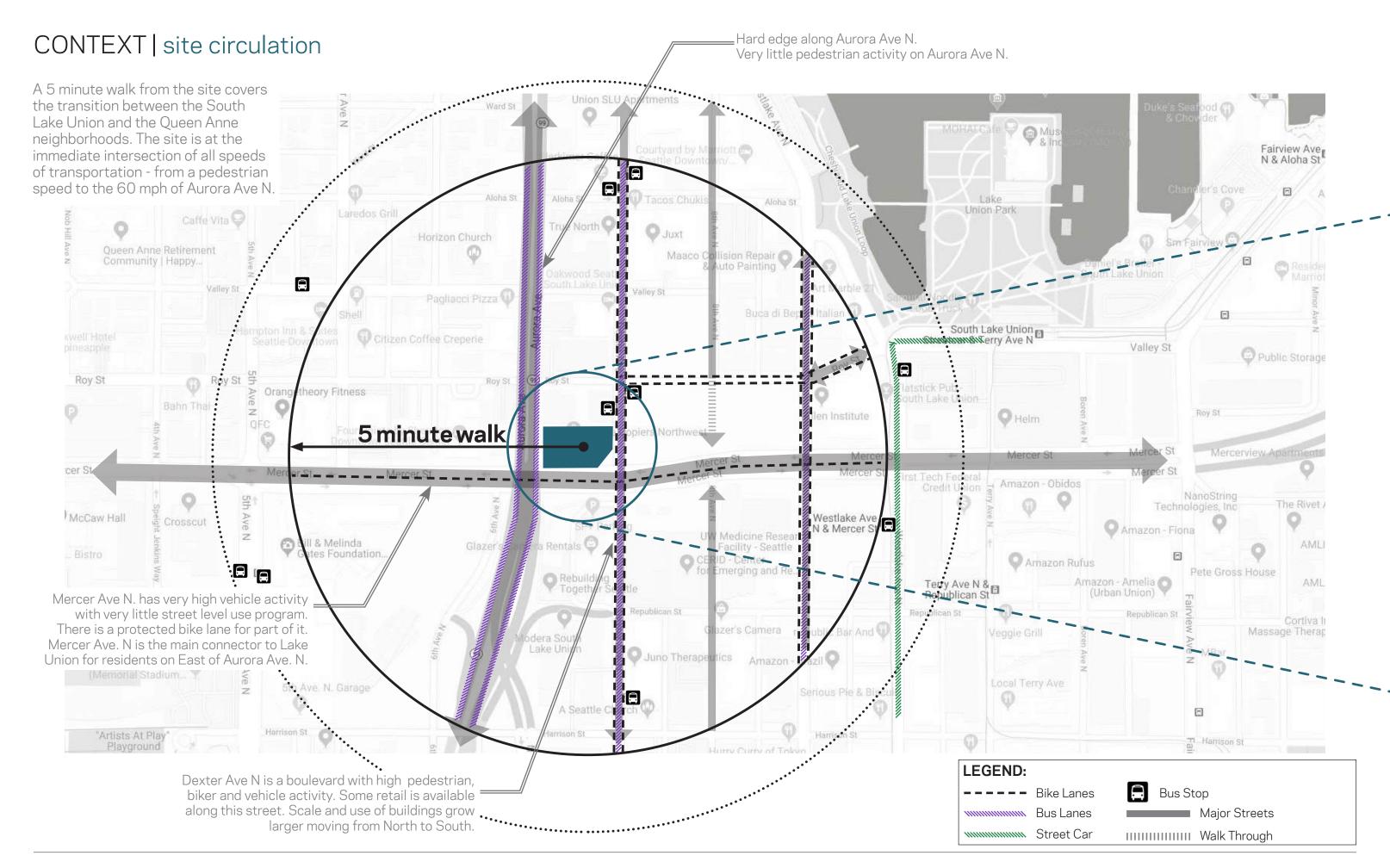


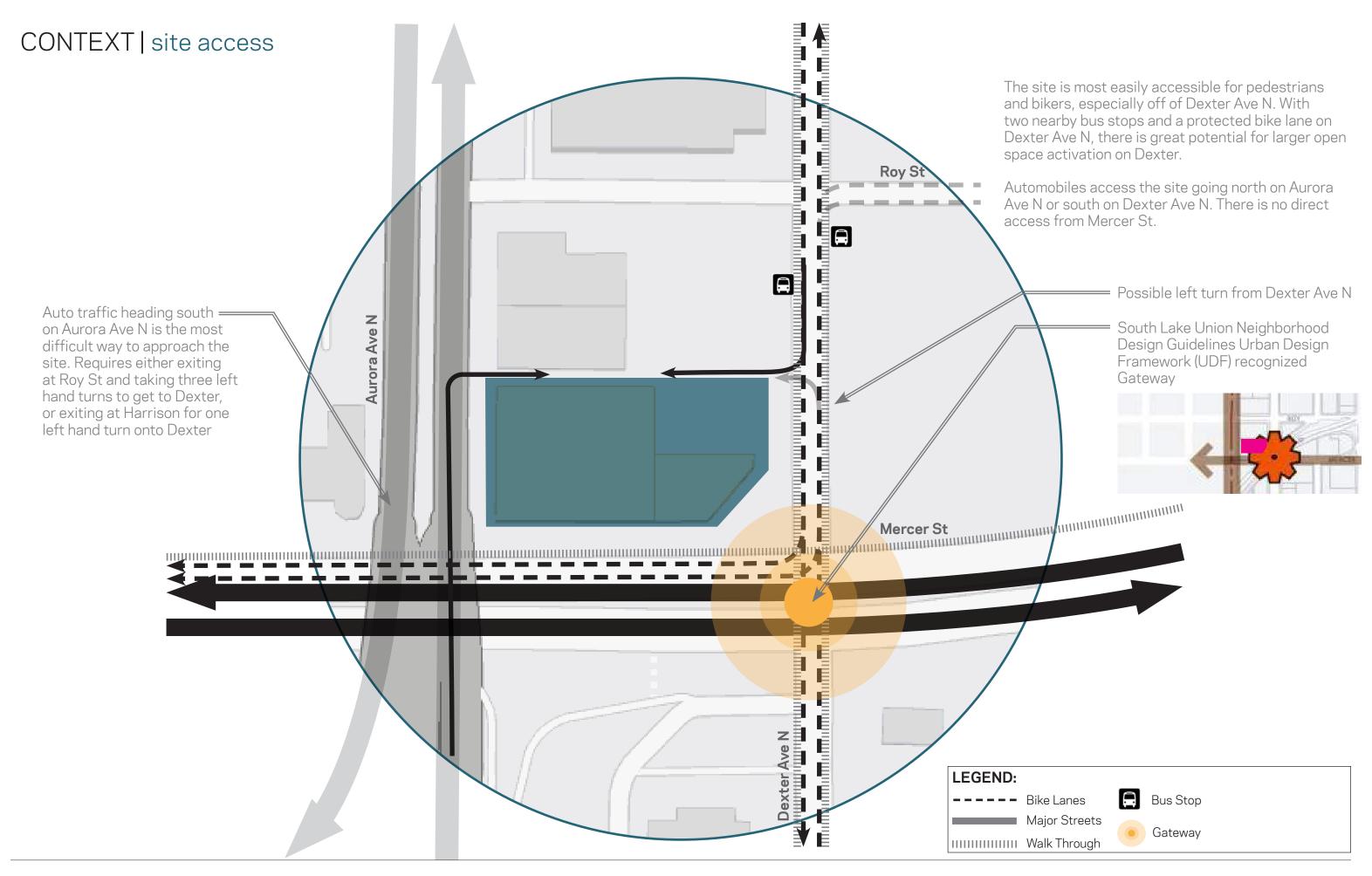
B. Opp Street View fronting Dexter Ave N.





C. Opp Street View fronting Aurora Ave N.





CONTEXT | intersection panorama



